



SAINT JOHN

PLANNING ADVISORY COMMITTEE

Tuesday, April 13, 2021, 6:00 P.M.

Meeting Conducted by Electronic Participation

Pages

1. Approval of the Agenda

2. Roll Call

3. Declaration of Conflict of Interest

4. Approval of the Minutes

There are no minutes to approve.

5. Applications

5.1. 1210 Loch Lomond Road - Section 59 Amendment

1 - 36

5.2. 300 Sydney Street - Rezoning & Variance

37 - 126

6. New Business

7. Next Meeting

8. Adjournment



The City of Saint John

Date: April 9, 2021
To: Planning Advisory Committee
From: Growth & Community Services
Meeting: April 13, 2021

SUBJECT

Applicant: 693383 NB Inc.
Landowner: 693383 NB Inc.
Location: 1210 Loch Lomond Road
PID: 55008593
Plan Designation: Stable Commercial
Zoning: General Commercial (CG)
Application Type: Section 59 Amendment
Jurisdiction: Common Council has requested the views of the Planning Advisory Committee concerning proposed amendments to the Section 39 conditions of the subject property. Council will consider the Committee's recommendation at a public hearing on **Monday, April 19, 2021**.

EXECUTIVE SUMMARY

The proposal involves the construction of a second commercial building on the front portion of the property located at 1210 Loch Lomond Road. The applicant is seeking approval to change the existing Section 39 conditions imposed on this portion of the property to allow for

construction of this second building and to use a portion of the building for contractor services and the storage of materials associated with their property management business.

Staff recommends the approval of the application as requested by the applicant. The staff recommendation will update the wording of the existing conditions to reflect the land use terminology and standards of the current Zoning By-law. A new condition requiring a traffic impact statement is recommended.

RECOMMENDATION

1. That Common Council, pursuant to the provisions of Section 59 of the *Community Planning Act*, rescind the conditions imposed on the November 6, 2006, rezoning of the parcel of land having an area of approximately 10,000 square metres, located at 1210 Loch Lomond Road, also identified as a portion of PID Number 55008593, and as amended on October 24, 2011 and December 23, 2013.
2. That Common Council, pursuant to the provisions of Section 59 of the *Community Planning Act*, impose the following conditions on the parcel of land having an area of approximately 10,000 square metres, located at 1210 Loch Lomond Road, also identified as a portion of PID Number 55008593:
 - (a) The use of the parcel of land be limited to the following: Artist or Craftsperson Studio, Business Office, Commercial Group, Health and Fitness Facility, Household Contractor Service, Library, Park or Playground, Personal Service, Restaurant with a Dining Room licence under the *Liquor Control Act*, and Retail General;
 - (b) The development of the parcel of land be in accordance with detailed building elevation and site plans, prepared by the proponent and subject to the approval of the Development Officer, illustrating the design and location of buildings and structures, garbage enclosures, outdoor storage, asphalt driveway access and parking areas enclosed with curbing, asphalt loading areas, landscaping, signs, exterior lighting, and other such site features;
 - (c) The above building elevation and site plans be attached to the permit applications for the development of the parcel of land;
 - (d) Driveway construction be subject to the approval of the Chief City Engineer;
 - (e) The parcel be developed and maintained with municipal water and municipal sanitary, subject to the approval of the Chief City Engineer;
 - (f) Exterior lighting be designed and maintained in a manner that does not direct light onto adjacent residential properties;

- (g) All disturbed areas of the site not occupied by buildings, parking areas, and driveways be landscaped within six months of the issuance of a development and building permit for any phase of the commercial development;
- (h) Except for fencing, no structure be permitted within 10 metres of the common property line with the adjacent property at 1204 Loch Lomond Road;
- (i) An all-season buffer of coniferous trees or a solid board-on-board fence be constructed and maintained along the common property line between the commercial development and the dwelling at 1204 Loch Lomond Road, subject to the approval of the Development Officer;
- (j) Any area designed for the storage of refuse or composting be screened by a solid board-on-board fence;
- (k) There be no outdoor storage of materials in association with the Household Contractor Service business; and
- (l) That a traffic impact statement be submitted by the applicant, subject to the approval of the City Traffic Engineer, prior to the issuance of a development and building permit for the proposed commercial building, and that any recommendation from the traffic impact statement be the responsibility of the applicant.

DECISION HISTORY

On October 17, 2006, the Planning Advisory Committee considered a rezoning application from Galbraith Florists Ltd. seeking permission to rezone the front portion of the property at 1210 Loch Lomond Road from "RS-2" One and Two Family Suburban Residential to "B-2" General Business to construct a commercial building containing a florist shop, hair salon, and two other businesses yet to be determined at the time.

Community Planning staff recommended denial of this rezoning application as the proposed commercial development was a variance to the former Municipal Plan. The Planning Advisory Committee recommended the rezoning application, subject to a Section 39 resolution to be prepared by staff prior to third reading.

On November 6, 2006, Common Council rezoned the subject area with the following conditions pursuant to Section 39 of the *Community Planning Act*:

- (a) The use of the property be limited to the following uses: antique shop or boutique, artist's or photographer's studio, barber shop or beauty salon, business office, dance or yoga studio, florist or gift shop, health club, retail store, institution of an educational, governmental, or philanthropic nature, or a public library, park, or playground;

- (b) The development be in accordance with detailed site and elevation plans approved by the Development Officer illustrating building design, asphalt parking areas, access, curbing, landscaping, and lighting;
- (c) The site be developed in accordance with detailed driveway construction and drainage plans, subject to the approval of the Chief City Engineer;
- (d) The site be provided with water and sanitary sewer services in a manner approved by the Chief City Engineer;
- (e) Exterior lighting be designed and placed in a manner that does not direct light onto adjacent residential properties;
- (f) All disturbed areas of the site not occupied by buildings, parking areas, and a driveway be landscaped within six (6) months of the issuance of a building permit for any phase of the commercial development;
- (g) No structures, except fencing, be permitted within 10 metres (33 feet) of the northwest property boundary;
- (h) An all-season buffer of either coniferous trees or a solid board-on-board fence, as approved by the Development Officer, be constructed along the northwest property boundary between the proposed development and the dwelling at 1204 Loch Lomond Road; and
- (i) Any areas designed for the storage of refuse or composting be screened by a solid board-on-board fence.

On October 18, 2011, the Committee considered a Section 39 Amendment application by Dianna Pegg seeking permission to establish a restaurant (diner) in the existing commercial building.

Staff did not recommend the amendment due to concerns that the land use could adversely affect the neighbouring residential properties. The Committee recommended the Section 39 Amendment application and recommended the following conditions for the restaurant:

- (a) That the hours of operation for said diner be limited to 8 a.m. to 7 p.m., Monday to Saturday;
- (b) That a liquor license not be permitted for the proposed restaurant;
- (c) That the use of a restaurant at the subject site be limited to the current application;
- (d) That the applicant be required to establish a fenced-in garbage area in the rear yard of the subject property; and

- (e) That the landscaped plantings be extended along the southern part of the property to the public right-of-way.

On October 24, 2011, Council approved the Section 39 Amendment application with the conditions recommended by the Committee.

On December 10, 2013, the Committee considered a Section 39 Amendment application by Kyung Po Park to permit a restaurant with a Dining Room liquor licence and extended hours of operation between 6 a.m. and 10:00 p.m., seven days a week.

Community Planning staff supported the application and recommended the following changes to the existing Section 39 conditions for a restaurant:

- (a) That the hours of operation be limited to 6 a.m. to 10 p.m., seven days a week;
- (b) That a Dining Room liquor license be permitted for the proposed restaurant;
- (c) That the applicant be required to establish a fenced-in garbage area in the rear of the subject property; and
- (d) That the landscaped plantings be extended along the southern part of the property to the public right-of-way.

The Committee recommended the staff Section 39 conditions to Council, except the hours of operation which were changed to 6 a.m. and 9 p.m.

On December 23, 2013, Council approved the Section 39 Amendment application with the conditions recommended by the Committee, except condition (a) relating to the hours of operation which was not imposed.

ANALYSIS

Proposal

The applicant is proposing to construct a second commercial building on the front portion of the subject property in behind an existing commercial building (Submission 1). The existing paved driveway, parking, and loading areas would be extended to include the building site of the new building.

If approved, the larger office space and the three garage bays in the new building will be used by 3D Property Management Inc. (Submission 2). The bays will be used for contractor services and the storage of materials associated with the property management business, which employs 32 people, seven of which will be working full time in the building. The applicant has indicated that no more than three customers usually visit the office at any one time, and that hours of operation are between 8:30 a.m. and 5:00 p.m., weekdays (excluding holidays). The

applicant also indicated that there would be no outdoor storage of materials. The remaining areas of the building would be occupied with two business offices.

Site and Neighbourhood

The subject property is located on the south side of Loch Lomond Road between Hickey Road and Bon Accord Drive in East Saint John (Map 1). The property has an area of 25,578 square metres and a street frontage of 80 metres. The Little River is located along the rear of the property and a brook flows over the eastern portion of the property to this river.

An existing commercial building is situated on the front portion of the subject property, 23 metres from the front property line. This building initially contained a restaurant, florist shop, and hair salon. Presently, the building contains a vape shop business (Vapires), a constituency office for Glen Savoie, MLA, an office for Ambulance New Brunswick, and an office for a mortgage broker. The site has been developed with landscaping and a paved driveway, parking area, and loading area. There is board-on-board fencing between the commercial building and the dwelling at 1204 Loch Lomond Road (a portion of this fencing needs repair). An enclosed refuse area is located at the rear of the existing building (site).

This portion of Loch Lomond Road is designated as an arterial street. The immediate area surrounding the subject property contains a mix of residential and commercial land uses. There are eight single detached dwellings to the west on both sides of Loch Lomond Road. Across the street is the Loch Lomond Irving Convenience Store and Gasoline Bar (1233 Loch Lomond Road) and the Loch Lomond Mitsubishi Dealership (1265 Loch Lomond Road). Further to the west along the southside of the street is The Power Store (1120 Loch Lomond Road), Loch Lomond Home Hardware (1120 Loch Lomond Road), and a Tim Hortons restaurant (1100 Loch Lomond Road).

Section 59 Amendment Application

In 2006, the front portion of the subject property was rezoned to permit the construction of a commercial building. Section 39 conditions were imposed on the rezoning that limited the development to certain commercial land uses (Decision History). Conditions were also imposed that regulated site development, building construction, and servicing. These conditions were amended in 2011 and 2013 to permit a restaurant with a dining room liquor license.

The new landowner is seeking approval to amend the existing Section 39 conditions to allow a Household Contractor Service¹ business (i.e., contractor services and the storage of materials associated with the property management business) and to allow a Commercial Group (i.e., two commercial buildings developed with shared site facilities).

¹ A Household Contractor Service means an establishment that provides electrical, heating, painting, plumbing, or similar small-scale contractor service primarily to individual households, and includes the accessory sale of goods provided all materials are kept within an enclosed building and there is no manufacturing involved.

Municipal Plan

In 2011, the Municipal Plan introduced a new planning framework for the area. The properties along the south side of Loch Lomond Road between Hickey Road and the subject site, and the properties across the street from the subject site, have been designated Stable Commercial (Map 2). This general area was previously designated Low Density Residential by the former Municipal Plan. The front portion of the subject property, where the existing and proposed commercial buildings are located, is now within this new commercial designation.

Stable Commercial designated areas are lands which include a number of existing neighbourhood commercial areas that are usually built out and are not anticipated to experience major change over the planning period (2011-2035), but these areas have the potential for greater mixed-use development beyond the planning period.

Municipal Plan Policy LU-69 permits redevelopment of such designated properties, provided they can be developed in compliance with the standards of the Zoning By-law and do not negatively affect surrounding land uses.

The proposed development will satisfy the General Commercial (CG) zone standards (Map 3). The new commercial building will require 15 onsite parking spaces. Normally, a minimum side yard of 5 metres is required, however, an existing Section 39 condition requires a minimum side yard of 10 metres; a side yard of 18 metres will be provided for the proposed second commercial building.

The proposed Household Contractor Service business is not expected to negatively affect surrounding land uses, which are now transforming into commercial uses. The nearest residence at 1204 Loch Lomond Road will be approximately 36 metres from the new building.

The construction of a second commercial building on the subject site, referred to as a Commercial Group², will also not negatively affect surrounding land uses. The existing commercial building has an approximate ground floor area of 405 square metres. The construction of the proposed 500-square metre commercial building will only increase the commercial footprint to 905 square metres.

In comparison, the buildings for the Loch Lomond Irving Convenience Store and the Loch Lomond Mitsubishi Dealership have approximate ground floor areas of 819 square metres and 1,170 square metres, respectively. In addition, the proposed commercial building will be over 60 metres from the street and partially screened behind the existing commercial building.

Section 59 Recommended Conditions

Since the rezoning of the subject area in 2006, Section 39 conditions have been imposed on the use and development of this parcel. This application provides the opportunity to rescind these

² A Commercial Group means two or more commercial buildings located on a lot or adjoining lots that have been designed as a unified development with respect to the placement of buildings and any associated accessory buildings or structures, amenity spaces, driveways, landscaping, or parking areas.

conditions and readopt them under the new Section 59 provisions of the *Community Planning Act*. This will simplify the administration of these conditions.

In addition, with the adoption of a new Zoning By-law in 2014, a number of the land uses in the former by-law are named differently. The recommended Section 59 conditions will replace obsolete land use terms. All previously-approved land uses have been retained.

The Zoning By-law now includes a number of community standards to regulate the proposed development. However, a number of previous conditions have been retained in the recommendation to ensure that the existing development is regulated in the manner that has been approved by the City.

The City Traffic Engineer has expressed concern that there may be a need for an additional traffic lane to facilitate vehicles entering the commercial development. There is also concern about vehicles exiting the commercial development by a driveway that is in close proximity of two commercial driveways used by Loch Lomond Irving Convenience Store and Gasoline Bar across the street.

Therefore, the City Traffic Engineer is recommending that a Traffic Impact Statement be submitted by the applicant, and that the applicant be responsible for any recommendation determined by the Traffic Impact Statement to address these concerns. This matter has been included as a Section 59 condition.

Infrastructure Development has indicated that site drainage will be regulated by the Drainage By-law, and that a separate municipal water service and a separate municipal sewer service is required for the proposed new commercial building from existing mains. A water meter is also required for commercial sites.

The Section 59 conditions (and previously Section 39 conditions) requires that the development be served with municipal water service and municipal sewer service in a manner approved by the Chief City Engineer. In this case, as mentioned, this will require separate service connections to the proposed commercial building.

The recommended Section 59 conditions include both the requested Household Contractor Service land use and the Commercial Group as requested by the applicant.

Conclusion

The construction of a second commercial building with the proposed land uses is appropriate for the Stable Commercial area. Section 59 conditions will ensure neighbourhood compatibility.

ALTERNATIVES AND OTHER CONSIDERATIONS

No other alternatives were considered.

ENGAGEMENT**Public**

In accordance with the Committee's Rules of Procedure, notification of the proposal was sent to landowners within 100 metres of the subject property on April 1, 2021. The rezoning was posted on the City of Saint John website on March 29, 2021.

APPROVALS AND CONTACT

Author	Manager/Senior Planner	Commissioner
Mark O'Hearn, CPT	Jennifer Kirchner, RPP, MCIP	Jacqueline Hamilton, MCIP, RPP

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Telephone: (506) 649-6075

Email: Mark.OHearn@saintjohn.ca

APPENDIX

Map 1: **Aerial Photography**

Map 2: **Future Land Use**

Map 3: **Zoning**

Attachment 1: **Site Photography**

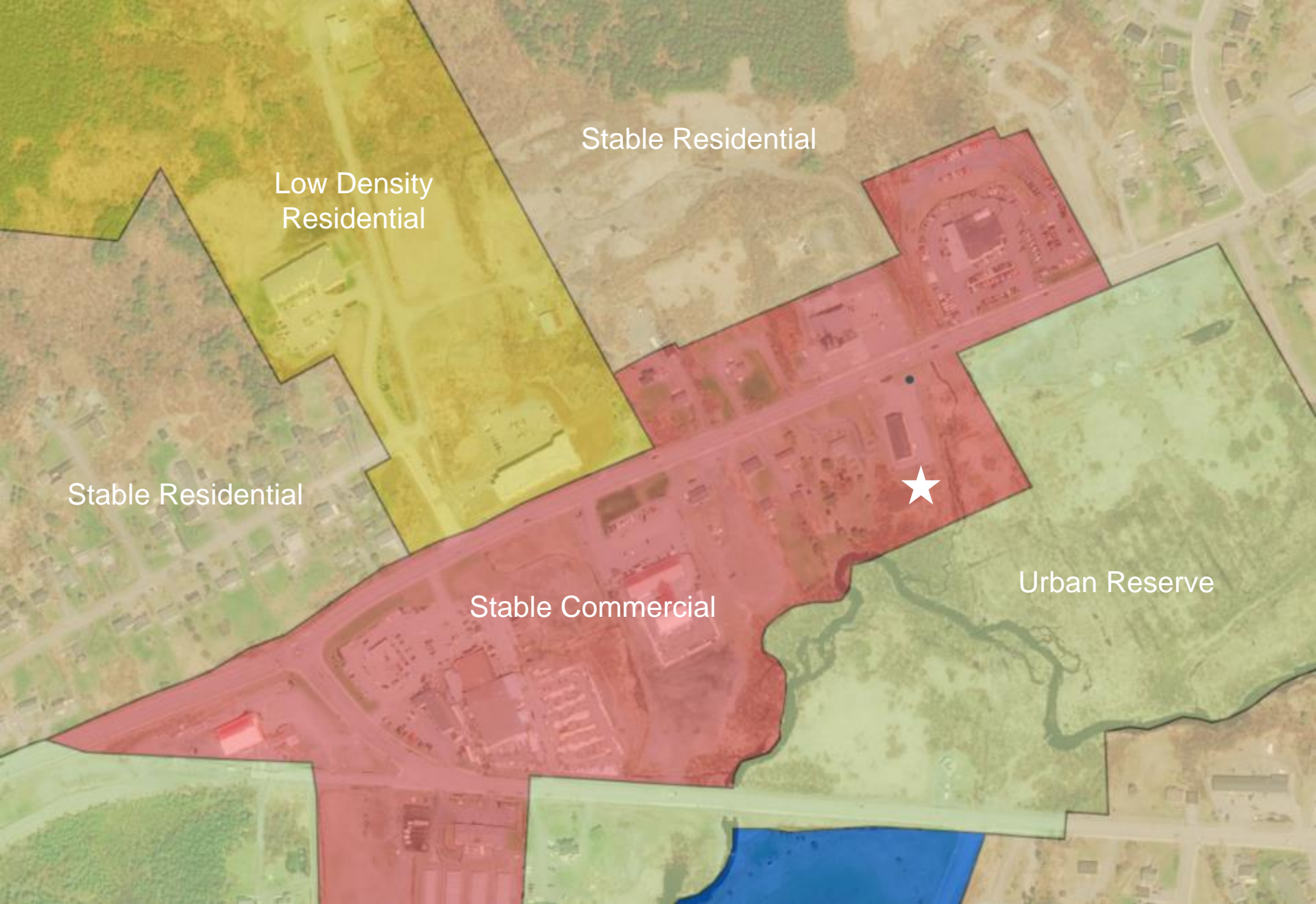
Attachment 2: **Municipal Plan Policy Review**

Submission 1: **Site Plan**

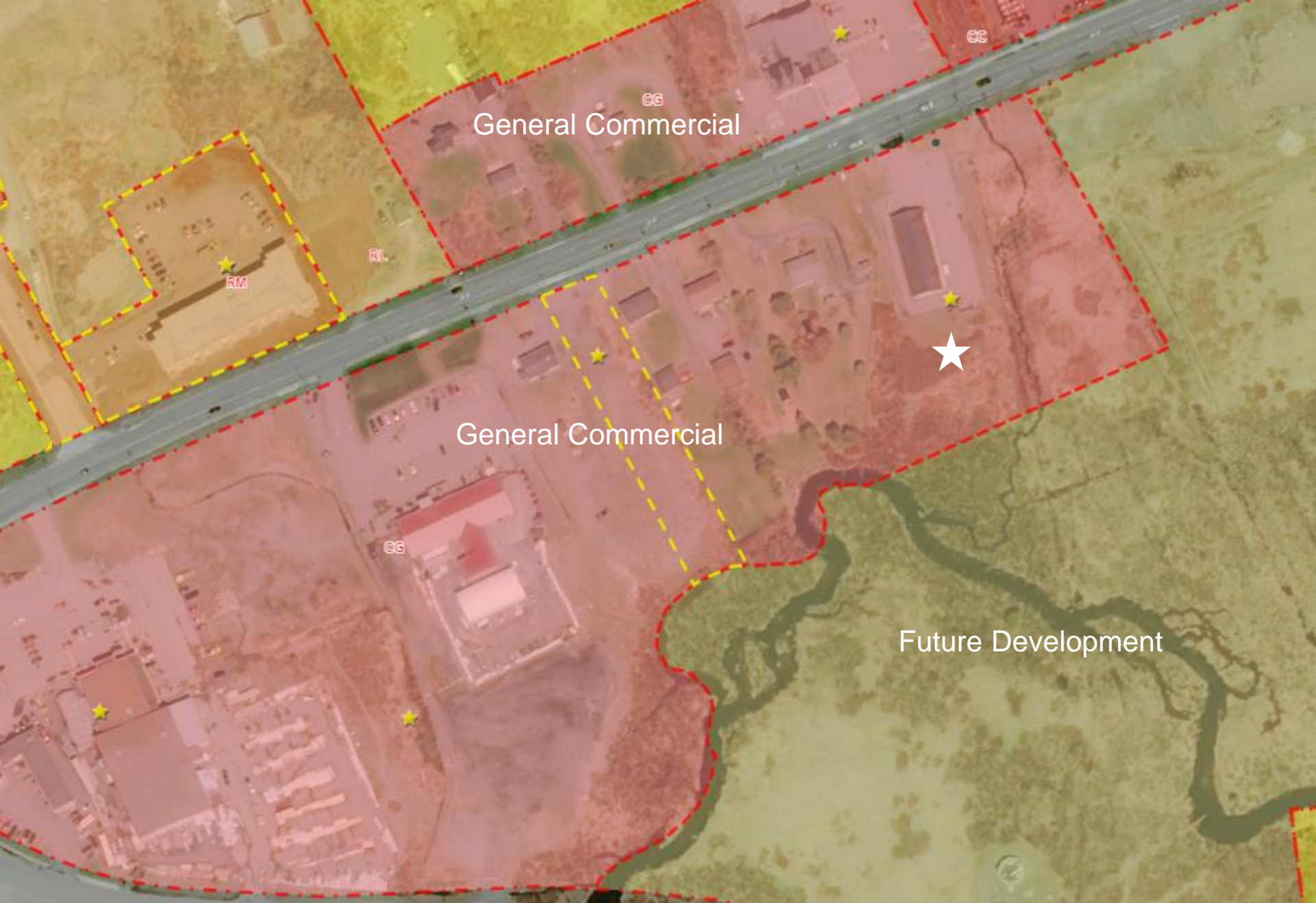
Submission 2: **Building Plans**



Map 1: Aerial¹⁰ Photography



Map 2: Future Land Use¹¹



Map 3:¹² Zoning



Attachment 1: ⁴³Site Photography

Attachment 2: Municipal Plan Policy Review

Policy	Staff Comment
<p>Stable Commercial</p> <p>These lands include a number of existing neighbourhood commercial areas which are generally built-out and are not anticipated to receive major change over the planning period. However, these Stable Commercial Areas have the potential for greater mixed-use development beyond the planning period.</p>	<p>The front portion of the subject property as designated Stable Commercial. In 2006, however, when it was rezoned commercially with Section 39 conditions, the area and neighbourhood was designated Low Density Residential.</p>
<p>Policy LU-69</p> <p>Create the Stable Commercial land use designation on the Future Land Use map (Schedule B). Within the Stable Commercial designation, a range of commercial uses exist which are likely to evolve over time. Within the Stable Commercial designation, redevelopment of property is permitted in compliance with the requirements of the Zoning Bylaw, provided the proposal does not negatively affect surrounding land uses. In addition, other compatible uses may be found in the Stable Commercial designation including housing, parks, and community facilities which are permitted in the designation without amendment to the Municipal Plan.</p>	<p>The proposed land uses, Household Contractor Service and Commercial Group, are both permitted uses in the General Commercial (CG) zone. These uses can be established in compliance with the standards of the Zoning By-law. There will not be any outdoor storage of materials.</p> <p>The proposed land uses are not expected to negatively affect the neighbourhood, which is now transforming with additional commercial development.</p> <p>The nearest residence, at 1204 Loch Lomond Road, will be approximately 36 metres from the proposed commercial building. The proposed building is located over 70 metres from Loch Lomond Road.</p>
<p>Policy LU-70</p> <p>Ensure that significant new development and redevelopment in areas designated Stable Commercial is permitted only when subject to a rezoning process, where compliance is demonstrated with the following criteria:</p> <p>a. The proposal does not detract from the City's intention to direct the majority of new commercial development to the Primary Centres, Regional Retail Centres, Business Centres, Commercial Corridors, Local Centres, and Mixed Use Centres;</p>	<p>The proposal involves the redevelopment of the front portion of the property from one commercial building to two commercial buildings with shared facilities (i.e., Commercial Group).</p> <p>The proposed development will only increase the existing commercial footprint of 405 square metres to approximately 905 square metres.</p>

Attachment 2: Municipal Plan Policy Review

Policy	Staff Comment
b. The proposed land use is desirable and contributes positively to the neighbourhood;	The inclusion of the proposed land uses is consistent with other commercial land uses in the area.
c. The proposal is compatible with surrounding land uses;	Neighbourhood compatibility can be achieved through Section 39 (59) conditions and the separation distances from the street and nearest dwelling.
d. The development proposal is in a location where all necessary water and wastewater services, protective services, and appropriate transportation infrastructure including public transit is provided;	The proposed commercial building will be served by municipal sewer and municipal water, and the property abuts Loch Lomond Road, a significant arterial street.
e. Site design features that address such matters as safe access, buffering and landscaping, site grading and stormwater management are incorporated; and	<p>The Section 39 (59) conditions and the new standards of the Zoning By-law will ensure that the proposed development site is properly developed and maintained. Site drainage will need to satisfy the requirements of the Drainage By-law.</p> <p>The proposed commercial building will be over 70 metres from the street and will not be integral to the Loch Lomond Road streetscape.</p>
f. A high quality exterior building design is provided that is consistent with the Urban Design Principles in the Municipal Plan.	The proposed building design and finishing is compatible with other commercial buildings in the area, including the existing commercial building on the subject site.

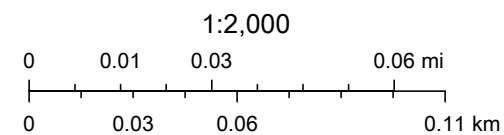
GeoNB Map Viewer

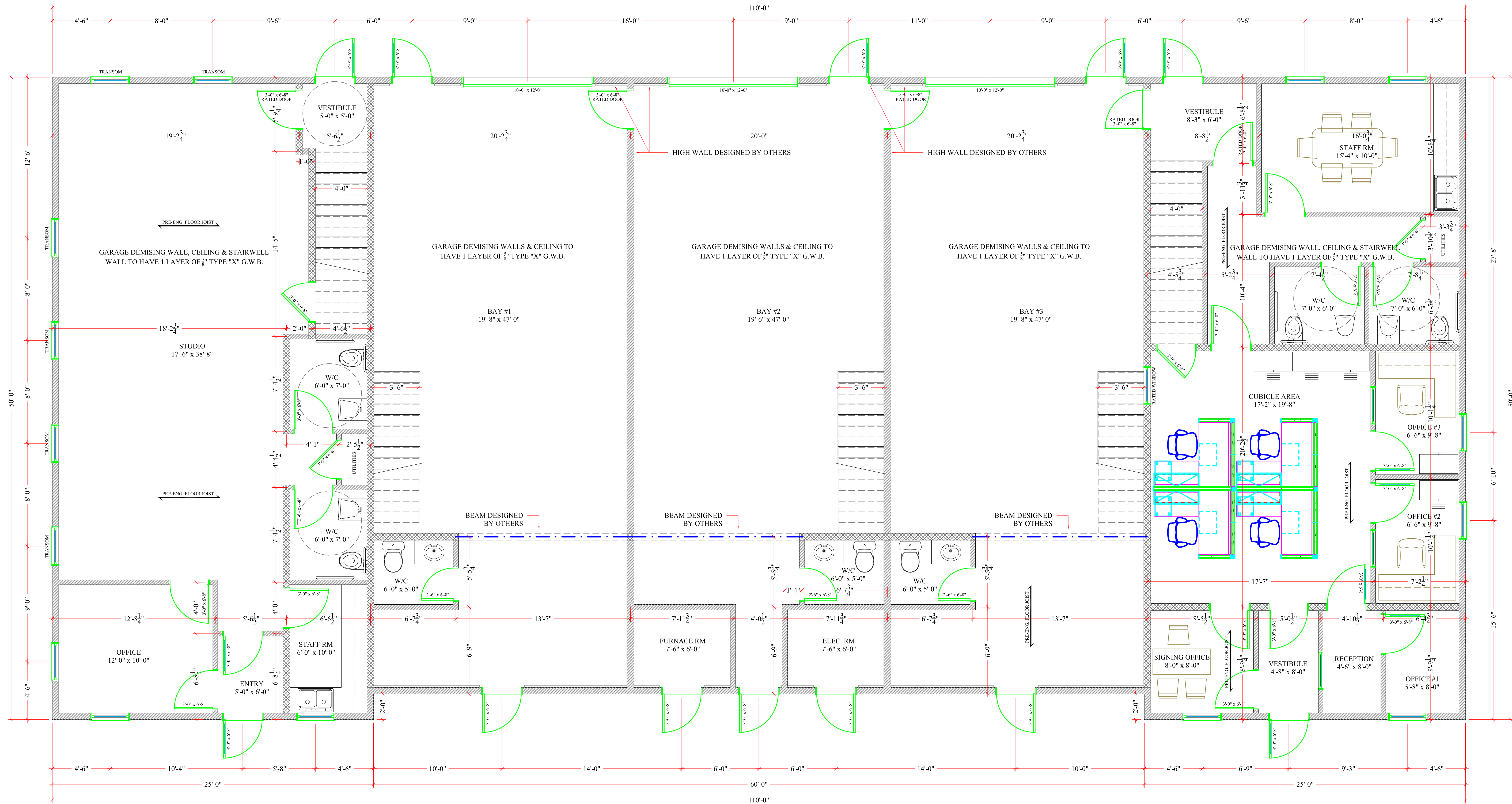


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Lines

- Override 3
- Override 1
- Override 2
- Georeferenced Civic Address Data Base (GCADB)
- parcels





1 GROUND FLOOR PLAN
A-1 SCALE: 1/4"=1'-0"
GROUND FLOOR BUILDING AREA = 5380 ft²

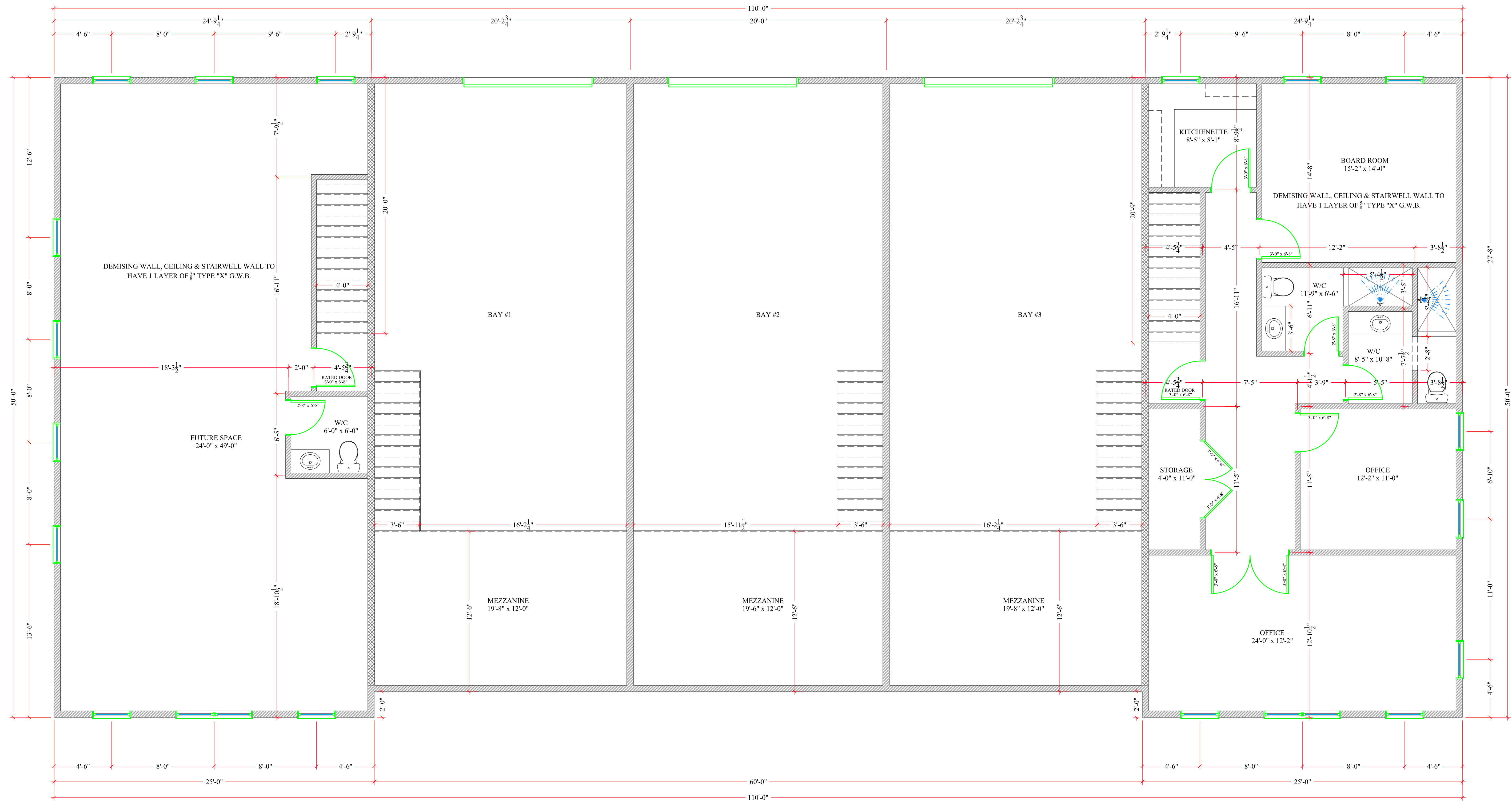
CONSTRUCTION NOTE:
ALL DIMENSIONS ARE FROM FACE OF STUD TO FACE OF STUD.
ALL LINTELS & HEADERS TO BE 3 PLY 2"x10" UNLESS NOTED OTHERWISE.
ALL DIMENSIONS TO BE FIELD CHECKED.
FOUNDATIONS TO MEET OR EXCEED THE CURRENT N.B.C. REQUIREMENTS.
ALL FRAMING TO MEET OR EXCEED THE CURRENT N.B.C. REQUIREMENTS.
ALL WINDOWS AND DOORS TO MEET ENERGY STAR FOR ZONE OF INSTALLATION.
AIR TIGHTNESS TO BE SEALED AND MAINTAINED THROUGHOUT ASSEMBLIES.
HRV SYSTEM MUST MEET MINIMUM REQUIREMENTS (N.B.C.C.).
SMOKE DETECTORS TO BE INSTALLED IN ALL BEDROOMS.
AIR / VAPOUR BARRIERS TO BE INSTALLED AS-PER N.B.C.C.
RADON MITIGATION SYSTEM TO BE INSTALLED AS-PER N.B.C.C.
ALL EXPANDED POLYSTYRENE WITH-IN BUILDING ENVELOPE TO BE SEGREGATED WITH 1/2" G.W.B.
ALL DECKS TO BE DESIGNED & DETAILED BY OTHERS SEALANT TO BE APPLIED BETWEEN BOTTOM PLATE & SUB-FLOOR ALONG PERIMETER.

BRACING NOTE:
ALL CEILINGS TO BE STRAPPED WITH A MINIMUM 1"x3" @ 16" o/c TO PROVIDE BOTTOM CORD BRACING.
ALL ROOF TRUSS BRACING SPECIFIED BY TRUSS SUPPLIER TO BE INSTALLED AS PER MANUFACTURERS INSTRUCTIONS.
ALL FLOOR SYSTEMS TO HAVE PERMANENT BRACING BETWEEN FLOOR JOISTS, AS PER MANUFACTURERS INSTRUCTIONS & RECOMMENDATIONS.
DIMENSIONAL LUMBER USED AS FLOOR JOISTS TO HAVE "X" BRACING AS PER CURRENT N.B.C.
CHECK WITH LOCAL BUILDING AUTHORITY FOR 9.23.13 (N.B.C.C.) REQUIREMENTS.

GENERAL NOTE:
THESE DRAWINGS ARE FOR GENERAL ARRANGEMENT ONLY. EACH & EVERY CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL CONSTRUCTION MEETS THE REQUIREMENTS OF THE CURRENT EDITION OF THE NATIONAL BUILDING CODE AND THE BYLAWS IN THE MUNICIPALITY OF CONSTRUCTION.
IT IS THE RESPONSIBILITY OF THE OWNER AND CONTRACTOR TO INSURE JOB SITE SAFETY MEETS THE CURRENT W.H.S.C.C. AND LOCAL BYLAWS.

LOAD BEARING INTERIOR WALL

0	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
 Specializing in Residential & Commercial Designs www.lockhartsdesign.ca PHONE: (506) 333-2202 FAX: (506) 333-2202			
PROJECT MOGUL MANAGEMENT			
TITLE GROUND FLOOR PLAN			
DESIGNED: B.L.	PLAN NO.	REV.	
DRAWN: B.L.	A-1		0
SCALE: AS NOTED			
DATE: OCT 2020			



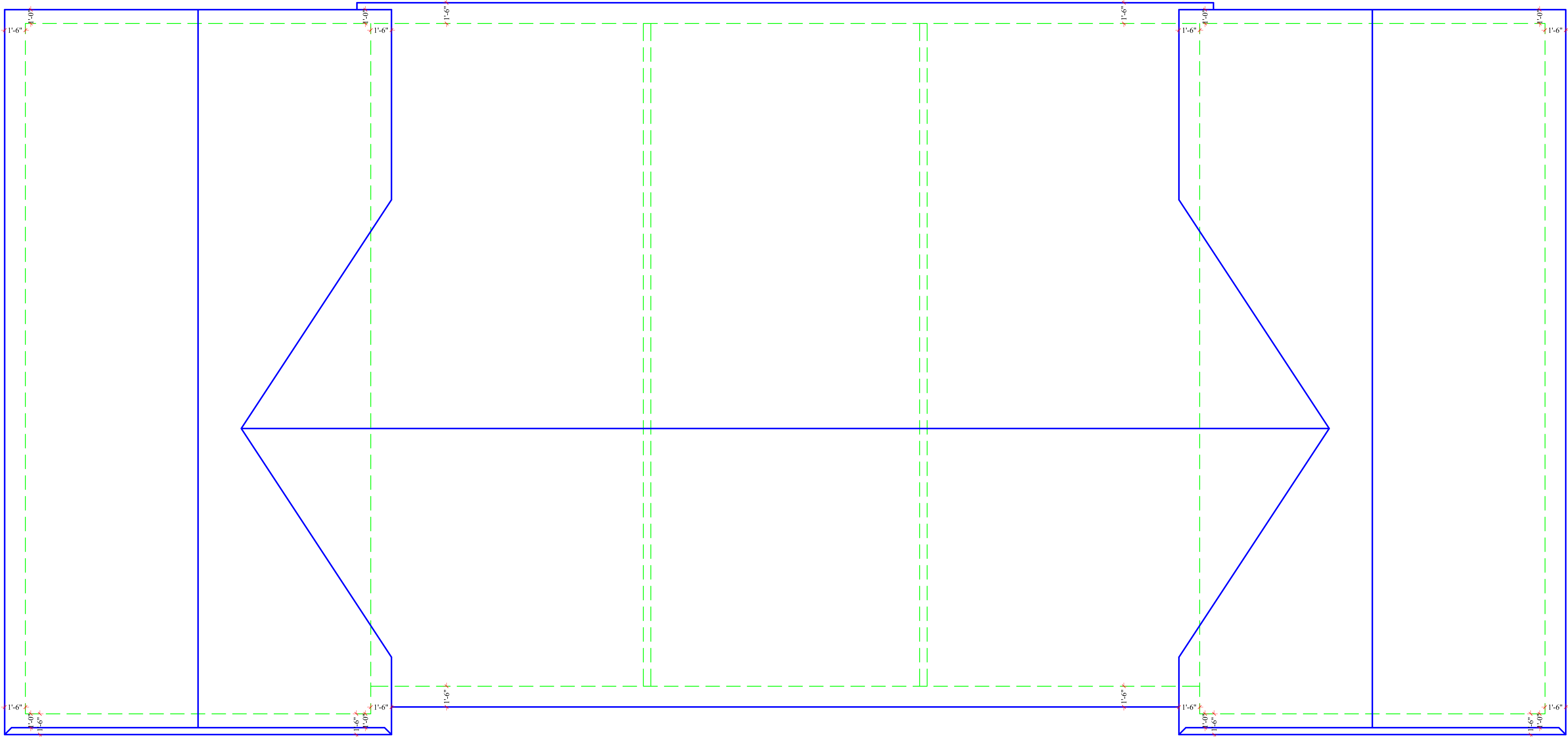
1 SECOND FLOOR PLAN
A-2 SCALE: 1/4"=1'-0"

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REV.	DESCRIPTION	BY	DATE

Specializing in Residential & Commercial Designs
www.lockhartsdesign.ca
PHONE: (506) 333-2202 EMAIL: lds@lockhartsdesign.ca

PROJECT		
MOGUL MANAGEMENT		

TITLE		
SECOND FLOOR PLAN		
DESIGNED: B.L.	PLAN NO.	REV.
DRAWN: B.L.	A-2	0
SCALE: AS NOTED		
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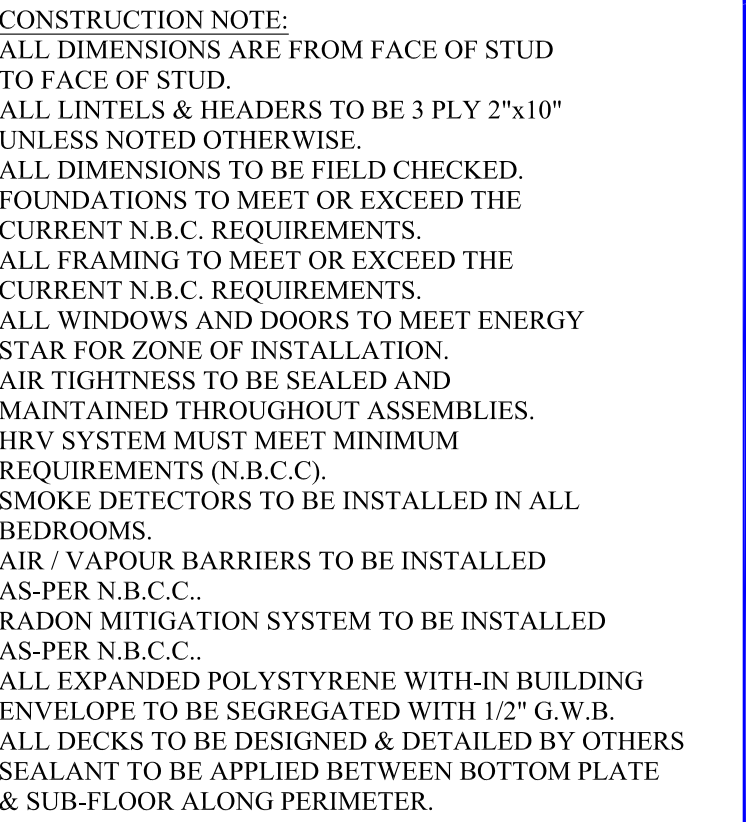
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CURRENT W.H.S.C.C. AND LOCAL BYLAWS.

 LOAD BEARING
 INTERIOR WALL

1 ROOF PLAN
A-3 SCALE: 1/4"=1'-0"

0	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
 Specializing in Residential & Commercial Designs www.lockhartsdesign.ca PHONE: (506) 333-2202 FAX: (506) 333-2202			
PROJECT MOGUL MANAGEMENT			

TITLE ROOF PLAN			
DESIGNED: B.L.	PLAN NO.	REV.	
DRAWN: B.L.	A-3		0
SCALE: AS NOTED			
DATE: OCT 2020			



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THE MUNICIPALITY OF CONSTRUCTION.
IT IS THE RESPONSIBILITY OF THE OWNER AND
CONTRACTOR TO INSURE JOB SITE SAFETY MEETS THE
CURRENT W.H.S.C.C. AND LOCAL BYLAWS.



0	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE



PROJECT

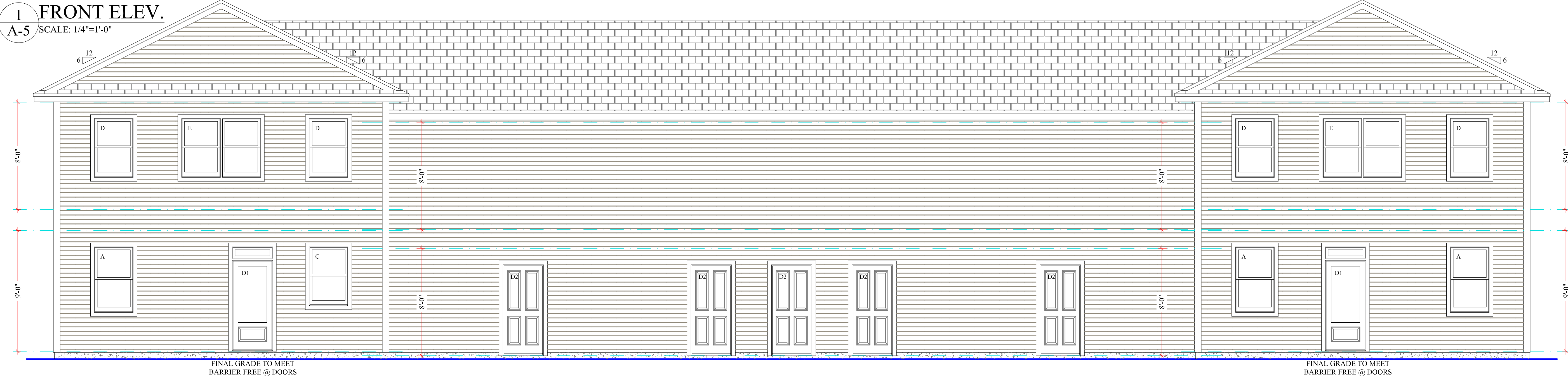
MOGUL MANAGEMENT

FOUNDATION PLAN

DESIGNED: B.L.	PLAN No. <div style="font-size: 2em; text-align: center;">A-4</div>	REV.
DRAWN: B.L.		<div style="font-size: 3em; text-align: center;">0</div>
SCALE: AS NOTED		
DATE: OCT 2020		

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1 FRONT ELEV.
A-5 SCALE: 1/4"=1'-0"



CONSTRUCTION NOTE:
ALL DIMENSIONS ARE FROM FACE OF STUD TO FACE OF STUD.
ALL LINTELS & HEADERS TO BE 3 PLY 2"x10" UNLESS NOTED OTHERWISE.
ALL DIMENSIONS TO BE FIELD CHECKED.
FOUNDATIONS TO MEET OR EXCEED THE CURRENT N.B.C. REQUIREMENTS.
ALL FRAMING TO MEET OR EXCEED THE CURRENT N.B.C. REQUIREMENTS.
ALL WINDOWS AND DOORS TO MEET ENERGY STAR FOR ZONE OF INSTALLATION.
AIR TIGHTNESS TO BE SEALED AND MAINTAINED THROUGHOUT ASSEMBLIES.
HRV SYSTEM MUST MEET MINIMUM REQUIREMENTS (N.B.C.C.).
SMOKE DETECTORS TO BE INSTALLED IN ALL BEDROOMS.
AIR / VAPOUR BARRIERS TO BE INSTALLED AS-PER N.B.C.C.
RADON MITIGATION SYSTEM TO BE INSTALLED AS-PER N.B.C.C.
ALL EXPANDED POLYSTYRENE WITH-IN BUILDING ENVELOPE TO BE SEGREGATED WITH 1/2" G.W.B.
ALL DECKS TO BE DESIGNED & DETAILED BY OTHERS SEALANT TO BE APPLIED BETWEEN BOTTOM PLATE & SUB-FLOOR ALONG PERIMETER.

BRACING NOTE:
ALL CEILINGS TO BE STRAPPED WITH A MINIMUM 1"x3"@16"o/c TO PROVIDE BOTTOM CORD BRACING.
ALL ROOF TRUSS BRACING SPECIFIED BY TRUSS SUPPLIER TO BE INSTALLED AS PER MANUFACTURES INSTRUCTIONS.
ALL FLOOR SYSTEMS TO HAVE PERMANENT BRACING BETWEEN FLOOR JOISTS, AS PER MANUFACTURES INSTRUCTIONS & RECOMMENDATIONS.
DIMENSIONAL LUMBER USED AS FLOOR JOISTS TO HAVE "X" BRACING AS PER CURRENT N.B.C. CHECK WITH LOCAL BUILDING AUTHORITY FOR 9.23.13 (N.B.C.C.) REQUIREMENTS

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LOAD BEARING INTERIOR WALL

2 LEFT ELEV.
A-5 SCALE: 1/4"=1'-0"

4 RIGHT ELEV.
A-5 SCALE: 1/4"=1'-0"

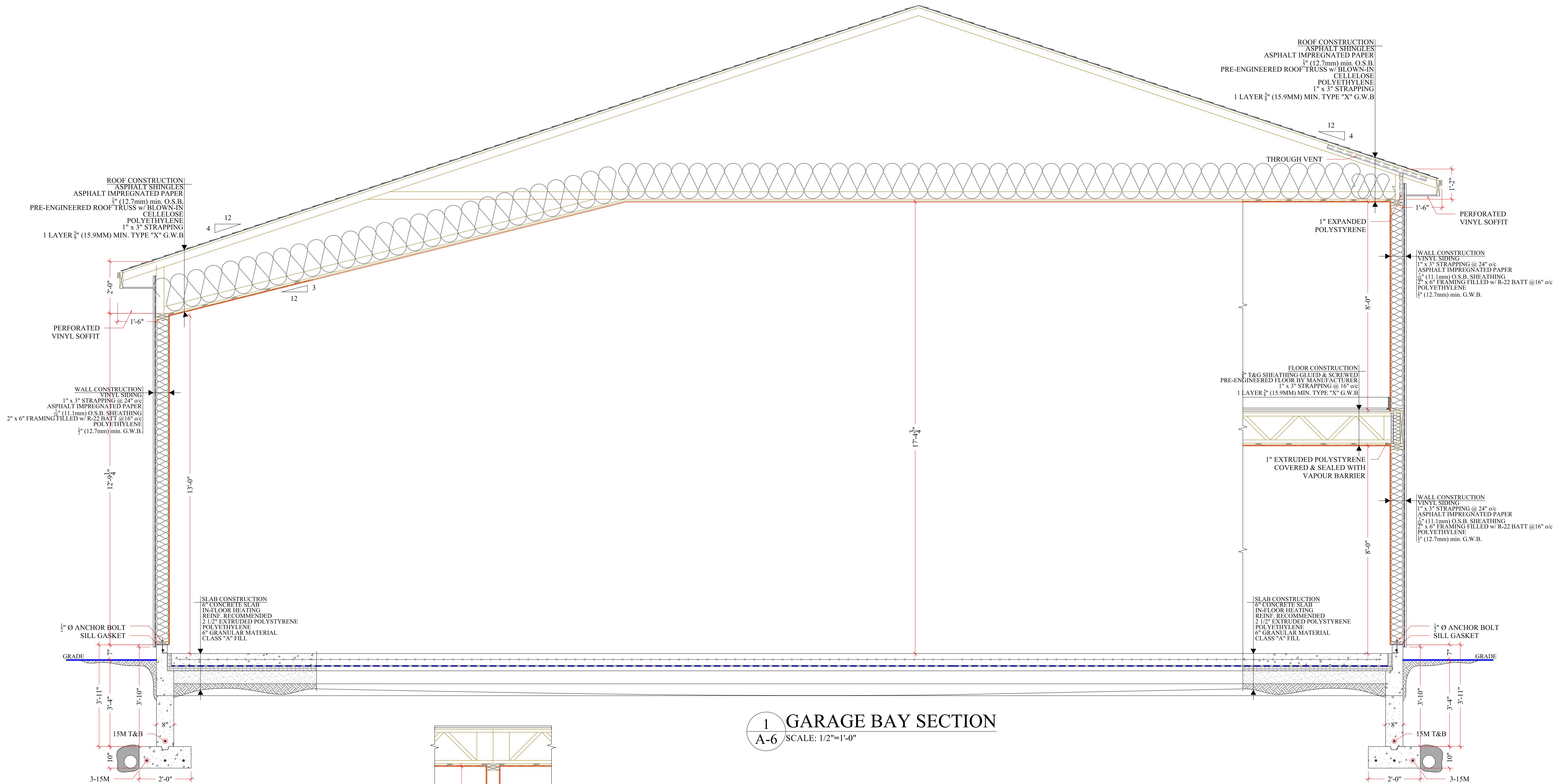
3 REAR ELEV.
A-5 SCALE: 1/4"=1'-0"



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0	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
 Specializing in Residential & Commercial Designs www.lockhartsdesign.ca PHONE: (506) 333-2282 EMAIL: info@lockhartsdesign.ca			
PROJECT MOGUL MANAGEMENT			

TITLE ELEVATIONS			
DESIGNED: B.L.	PLAN No.	REV.	
DRAWN: B.L.	A-5		0
SCALE: AS NOTED			
DATE: OCT 2020			

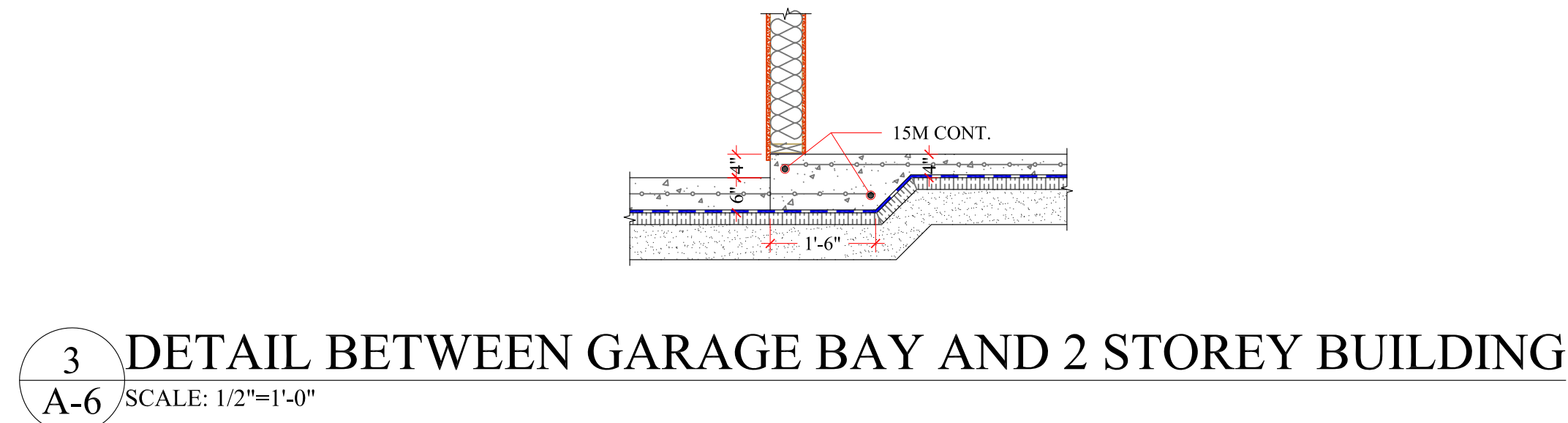
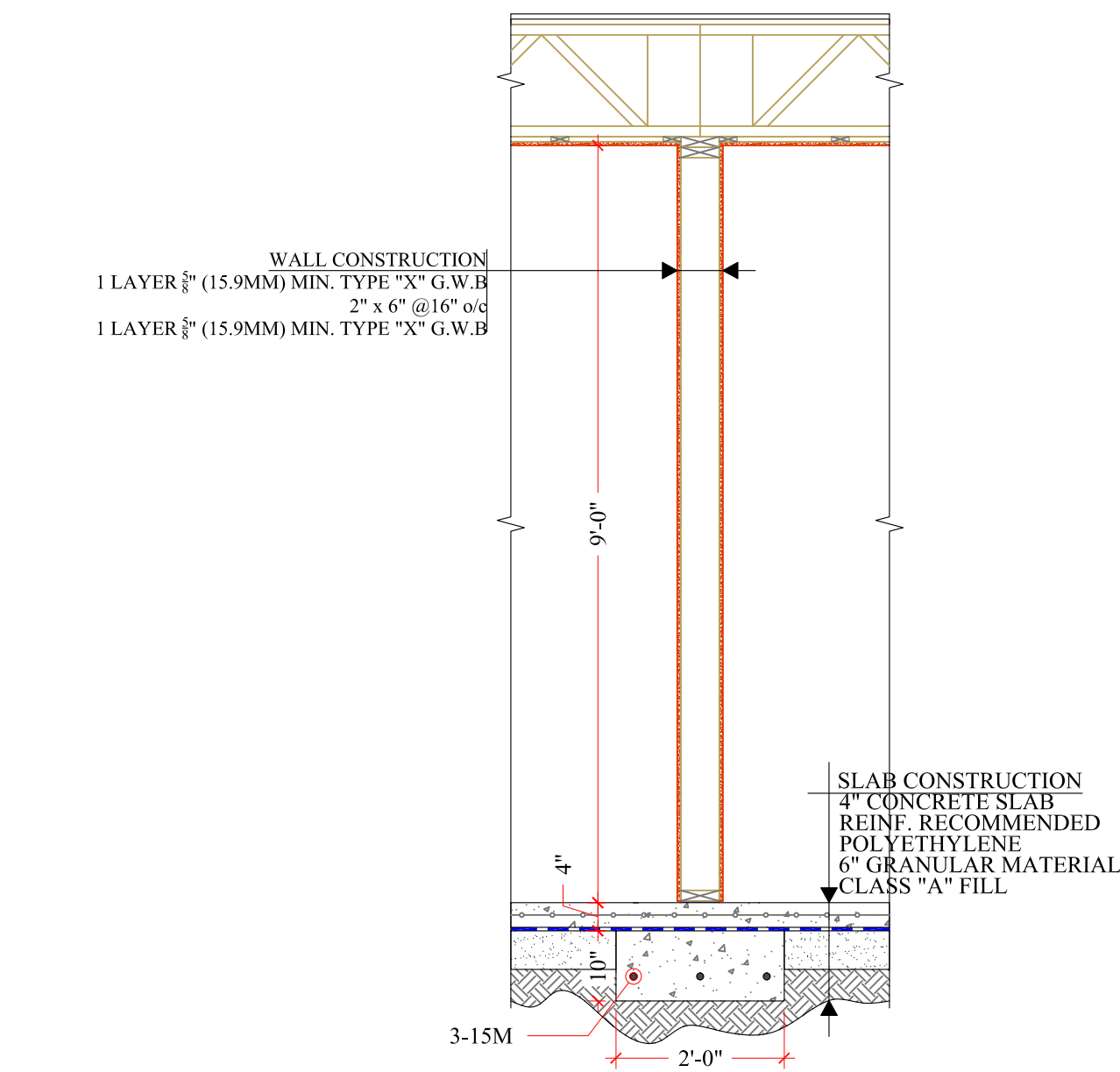


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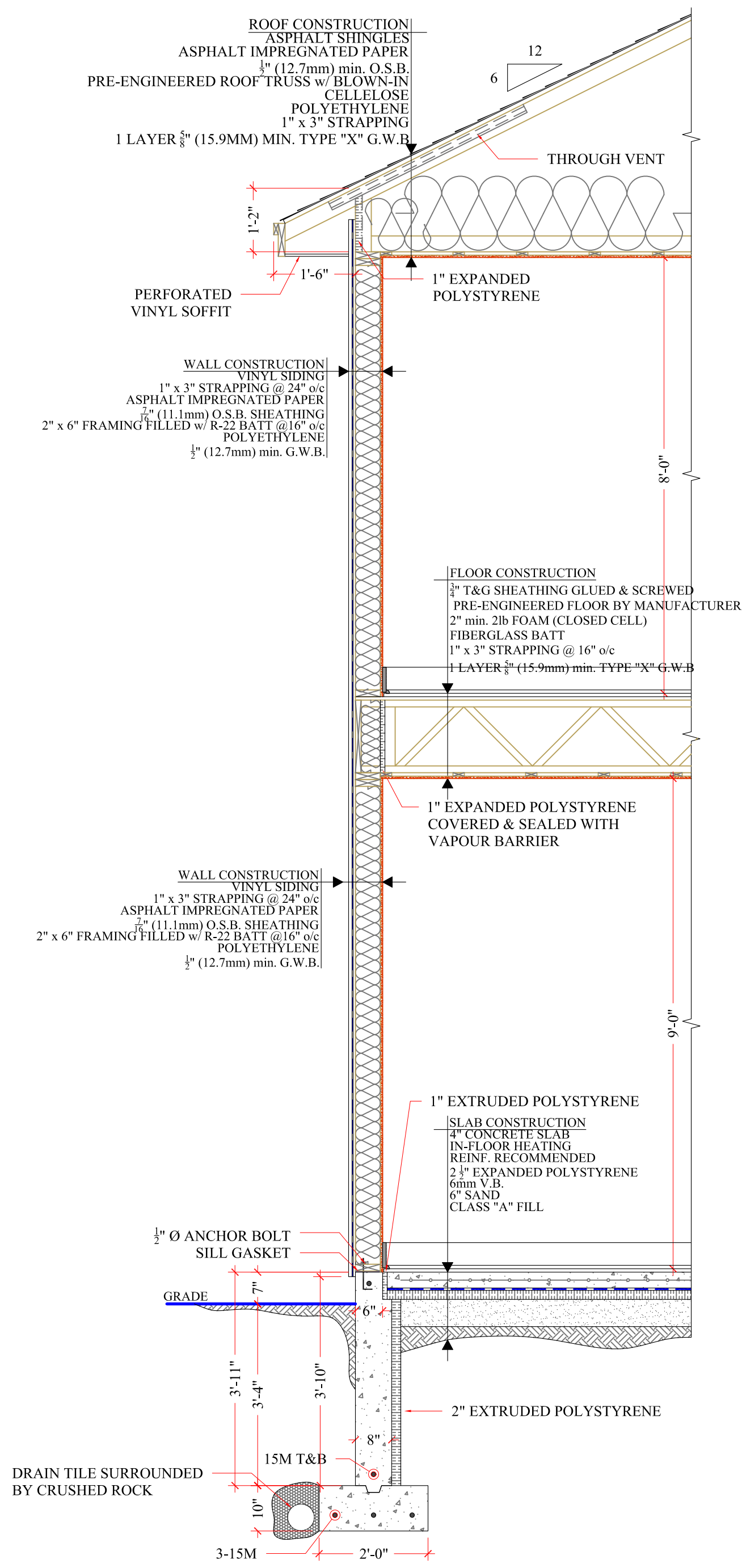
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LOAD BEARING
 INTERIOR WALL

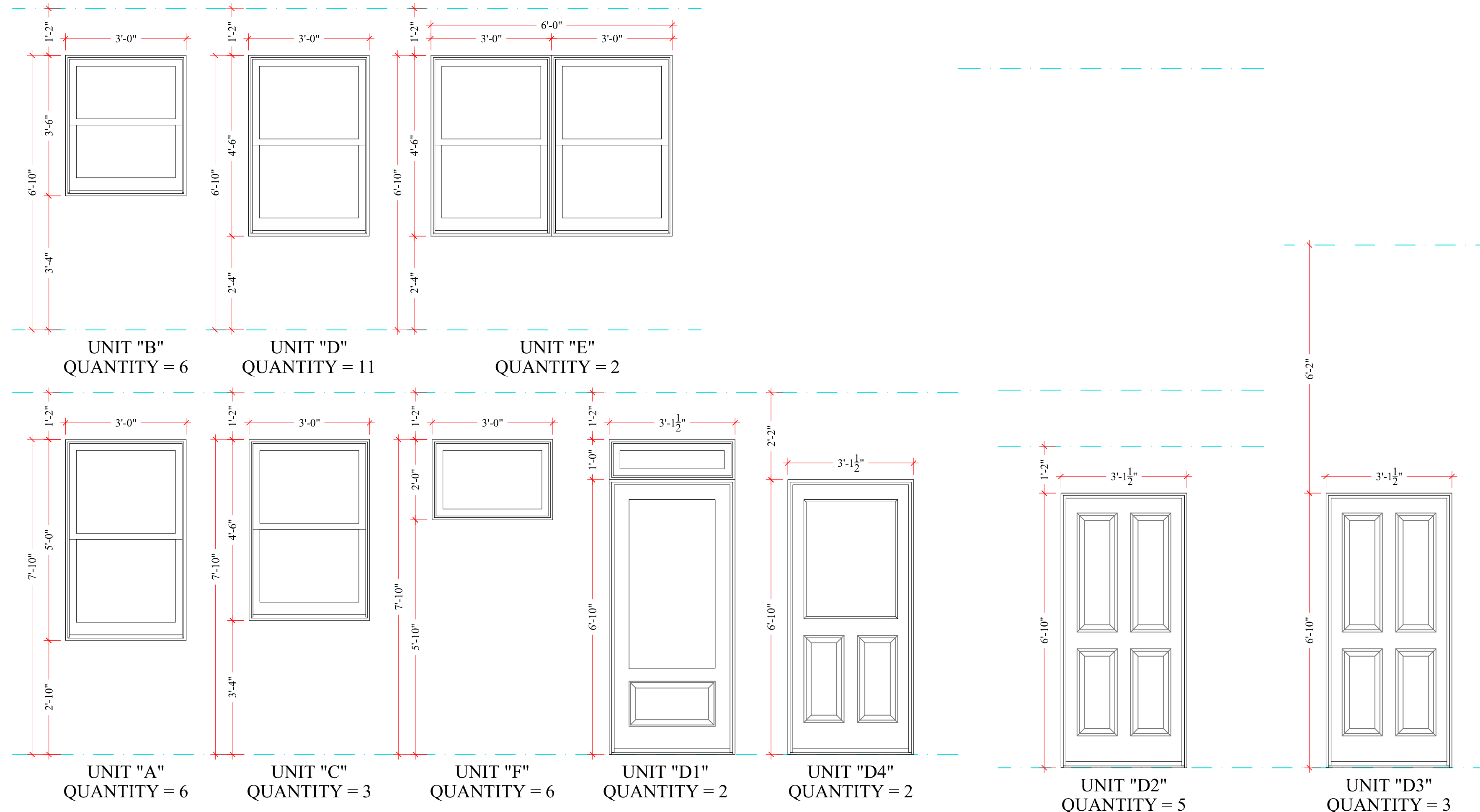


0	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
<p>Specializing in Residential & Commercial Designs www.lockhartsdesign.ca PHONE - (506) 333-2202 EMAIL - info@lockhartsdesign.ca</p>			
PROJECT			
MOGUL MANAGEMENT			

TITLE			
DETAILS			
DESIGNED: B.L.	PLAN NO.	REV.	
DRAWN: B.L.	A-6		0
SCALE: AS NOTED			
DATE: OCT 2020			



1
A-7 **2 STORY WALL DETAIL**
SCALE: 1/2"=1'-0"



2
A-7 **WINDOW & DOOR SCHEDULE**
SCALE: 1/2"=1'-0"

ROOF ASSEMBLY			
ROOF ASSEMBLY COMPONENTS	RSI VALUE	R VALUE	
1 INSIDE AIR FILM	0.11	0.03	
2 FINISH: 1 COAT LATEX PRIMER AND LATEX PAINT	0.00	0.00	
3 1/2" x 3/4" STRAPPING	0.00	0.00	
4 1/2" x 3/4" STRAPPING	0.13	0.03	
5 POLYETHYLENE	0.00	0.00	
6 PRE-ENGINEERED ROOF TRUSS w/ BLOWN-IN CELLULOSE	0.17	15.00	
7 ASPHALT IMPREGNATED PAPER	0.00	0.00	
8 1/2" x 3/4" STRAPPING	0.00	0.00	
9 1/2" x 3/4" STRAPPING	0.00	0.00	
10 ASPHALT IMPREGNATED PAPER	0.00	0.00	
11 EXTERIOR AIR FILM	0.03	0.17	
TOTAL	0.30	15.20	

ROOF ASSEMBLY CATHEDRAL			
CATHEDRAL ROOF ASSEMBLY COMPONENTS	RSI VALUE	R VALUE	
1 INSIDE AIR FILM	0.11	0.03	
2 FINISH: 1 COAT LATEX PRIMER AND LATEX PAINT	0.00	0.00	
3 1/2" x 3/4" STRAPPING	0.00	0.00	
4 1/2" x 3/4" STRAPPING	0.13	0.03	
5 POLYETHYLENE	0.00	0.00	
6 PRE-ENGINEERED ROOF TRUSS w/ BLOWN-IN CELLULOSE	0.17	15.00	
7 ASPHALT IMPREGNATED PAPER	0.00	0.00	
8 1/2" x 3/4" STRAPPING	0.00	0.00	
9 1/2" x 3/4" STRAPPING	0.00	0.00	
10 ASPHALT IMPREGNATED PAPER	0.00	0.00	
11 EXTERIOR AIR FILM	0.03	0.17	
TOTAL	0.30	15.20	

ABOVE GRADE WALL ASSEMBLY			
WALL ASSEMBLY COMPONENTS	RSI VALUE	R VALUE	
1 INSIDE AIR FILM	0.11	0.03	
2 FINISH: 1 COAT LATEX PRIMER AND LATEX PAINT	0.00	0.00	
3 1/2" x 3/4" STRAPPING	0.00	0.00	
4 1/2" x 3/4" STRAPPING	0.13	0.03	
5 POLYETHYLENE	0.00	0.00	
6 1/2" x 3/4" STRAPPING	0.00	0.00	
7 1/2" x 3/4" STRAPPING	0.00	0.00	
8 1/2" x 3/4" STRAPPING	0.00	0.00	
9 1/2" x 3/4" STRAPPING	0.00	0.00	
10 1/2" x 3/4" STRAPPING	0.00	0.00	
11 EXTERIOR AIR FILM	0.03	0.17	
TOTAL	0.30	15.20	

JOIST HEADER ASSEMBLY			
HEADER ASSEMBLY COMPONENTS	RSI VALUE	R VALUE	
1 INSIDE AIR FILM	0.11	0.03	
2 POLYETHYLENE	0.00	0.00	
3 1" x 3" EXPANDED POLYSTYRENE	0.06	1.76	
4 1" x 3" EXPANDED POLYSTYRENE	2.23	12.63	
5 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
6 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
7 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
8 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
9 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
10 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
11 EXTERIOR AIR FILM	0.03	0.17	
TOTAL	2.43	13.63	

IN-FLOOR HEATING SLAB ASSEMBLY			
SLAB ASSEMBLY COMPONENTS	RSI VALUE	R VALUE	
1 INSIDE AIR FILM	0.11	0.03	
2 1" x 3" EXPANDED POLYSTYRENE	0.00	0.00	
3 2" x 6" FRAMING FILLED w/ R-22 BATT @ 16" o/c	2.23	12.63	
4 POLYETHYLENE	0.00	0.00	
TOTAL	2.43	13.63	

WALKOUT FLOOR ASSEMBLY			
FLOOR ASSEMBLY COMPONENTS	RSI VALUE	R VALUE	
1 INSIDE AIR FILM	0.11	0.03	
2 FINISH: FLOORING	0.00	0.00	
3 4" CONCRETE SLAB	0.04	0.21	
4 2" EXPANDED POLYSTYRENE	1.76	10.09	
5 1" CONCRETE WALL	0.00	0.00	
6 1" CONCRETE WALL	0.00	0.00	
7 EXTERIOR AIR FILM	0.03	0.17	
TOTAL	2.93	16.63	

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LOAD BEARING INTERIOR WALL

0	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
 Lockhart's DESIGN & CONSULTING INC. Specializing in Residential & Commercial Designs www.lockhartsdesign.ca PHONE: (506) 333-2202 EMAIL: info@lockhartsdesign.ca			
PROJECT			
MOGUL MANAGEMENT			

TITLE			
DETAILS			
DESIGNED: B.L.	PLAN NO.	REV.	
DRAWN: B.L.			
SCALE: AS NOTED	A-7	0	
DATE: OCT 2020			

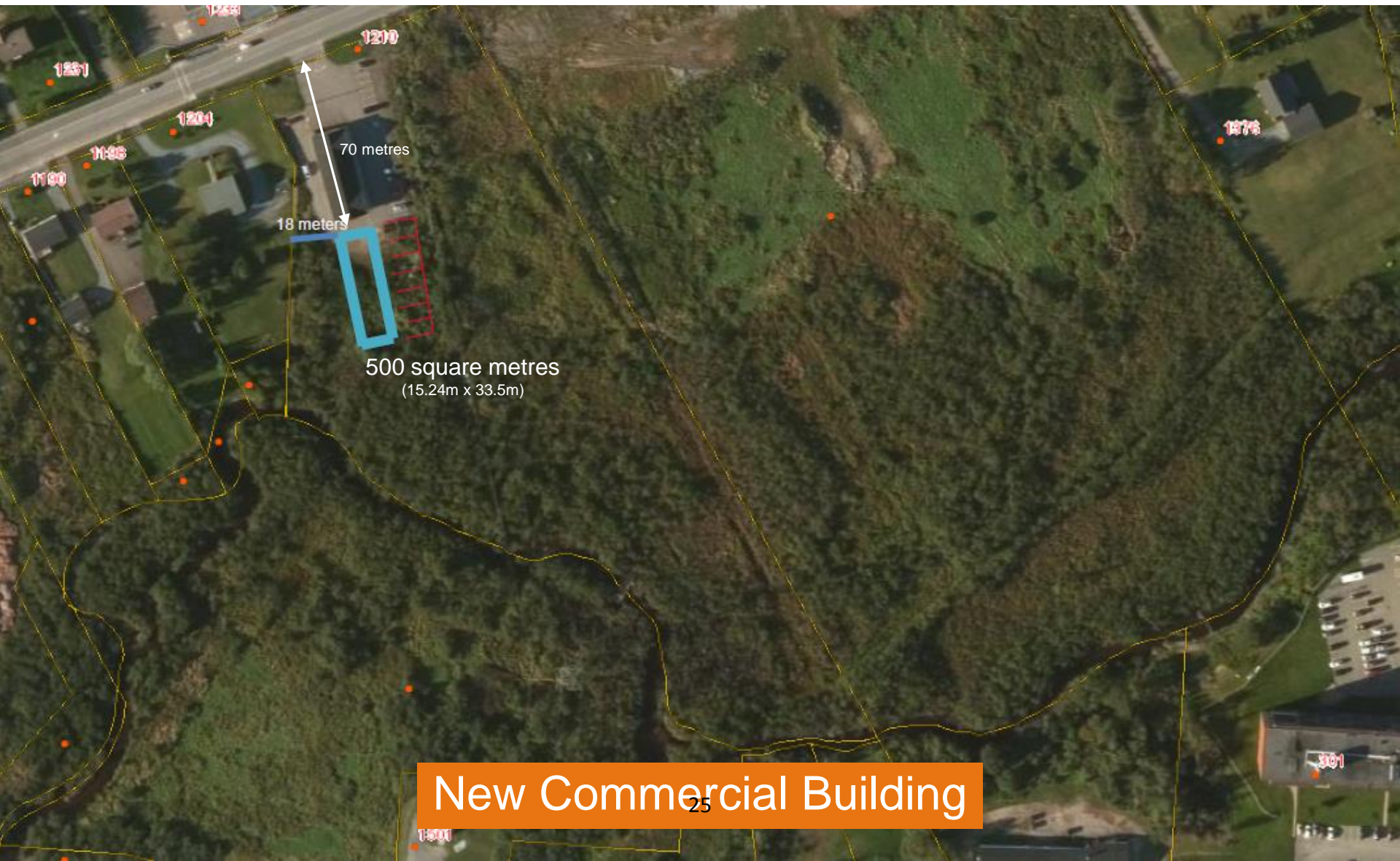


1210 Loch Lomond Road

Planning Advisory Committee Presentation

April 13, 2021

Proposal

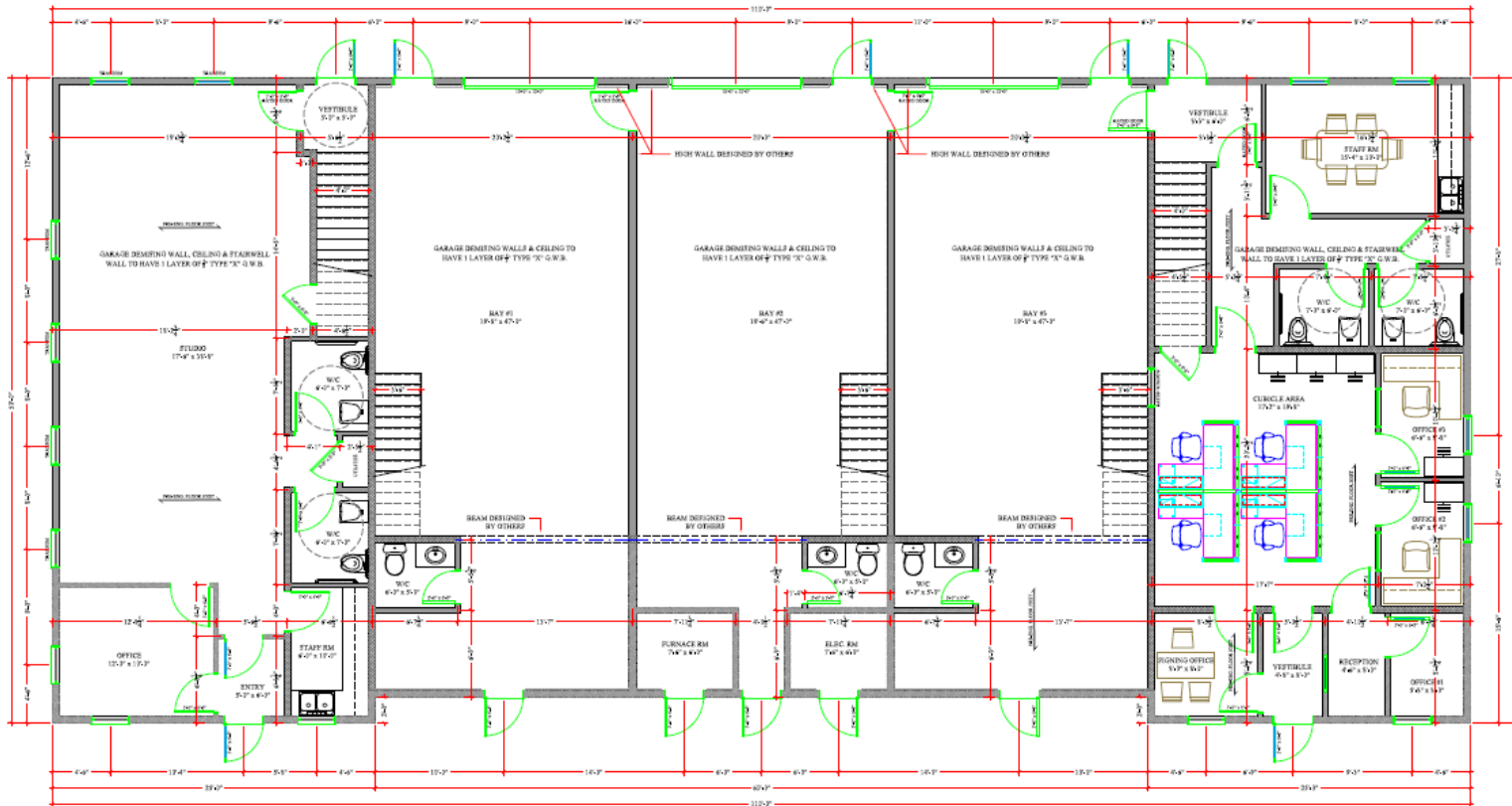


New Commercial Building

Proposal



Proposal



CONSTRUCTION NOTES:

SEE FOUNDATION PLAN AND FACE OF STUD TO FACE OF FIELD.

ALL EXTERIOR & INTERIOR TO BE 1/2" PLY 2417P UNLESS NOTED OTHERWISE.

ALL FOUNDATIONS TO BE FIELD CHECKED FOUNDATIONS TO MEET OR EXCEED THE CURRENT N.E.C. REQUIREMENTS.

ALL FRAMING TO MEET OR EXCEED THE CURRENT N.E.C. REQUIREMENTS.

ALL WINDOW AND DOOR TO MEET ENERGY STAR FOR 2009 OF INSTALLATION.

AIR TIGHTNESS TO BE SEALED AND MAINTAINED THROUGHOUT ASSEMBLY.

REV FLOOR MUST MEET MINIMUM REQUIREMENTS (N.E.C.).

SHALL BE INSTALLED TO BE INSTALLED IN ALL ROOMS.

ALL VARIOUS BARBERS TO BE INSTALLED AS PER N.E.C.

KIDNEY MITIGATION SYSTEM TO BE INSTALLED AS PER N.E.C.

ALL EXPANDED POLYSTYRENE WITHIN BUILDING DIVISION TO BE SEGREGATED WITH 2" G.W.B. ALL DUCT TO BE DETACHED & DETACHED BY OTHERS. SHALL BE APPLIED BETWEEN BOTTOM PLATE & TOP FLOOR ALONG PERIMETER.

BRACING NOTES:

ALL CEILING TO BE TRAPPED WITH A MINIMUM 1" x 1/2" x 1/4" TO PROVIDE BOTTOM CORN BRACING.

ALL 2" x 4" x 1/2" BRACING PROVIDED BY OTHERS. PUMP TO BE INSTALLED AS PER MANUFACTURER INSTRUCTIONS.

ALL FLOOR SYSTEMS TO HAVE PERMANENT BRACING BETWEEN FLOOR JOISTS AT PERMANENT MANUFACTURE AND PROTECT A SECONDARY DRAINAGE.

INDIVIDUAL LUMBER ORS AS FLOOR JOIST TO HAVE 2" x 4" BRACING AS PER CURRENT N.E.C. CHECK WITH LOCAL BUILDING AUTHORITY FOR N.E.C. REQUIREMENTS.

GENERAL NOTES:

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LOAD BEARING INTERIOR WALL

3	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
<p>Lockhart's Design & Consulting Inc.</p> <p>Building & Residential & Commercial Design</p> <p>PHONE: 416-490-1000 FAX: 416-490-1001</p> <p>WWW.LOCKHARTSDC.COM</p>			
PROJECT			
MOGUL MANAGEMENT			
TITLE			
GROUND FLOOR PLAN			
DESIGNED BY:	DATE:	SCALE:	0
DRAWN BY:	DATE:	SCALE:	0
CHECKED BY:	DATE:	SCALE:	0
APPROVED BY:	DATE:	SCALE:	0

Site & Neighbourhood

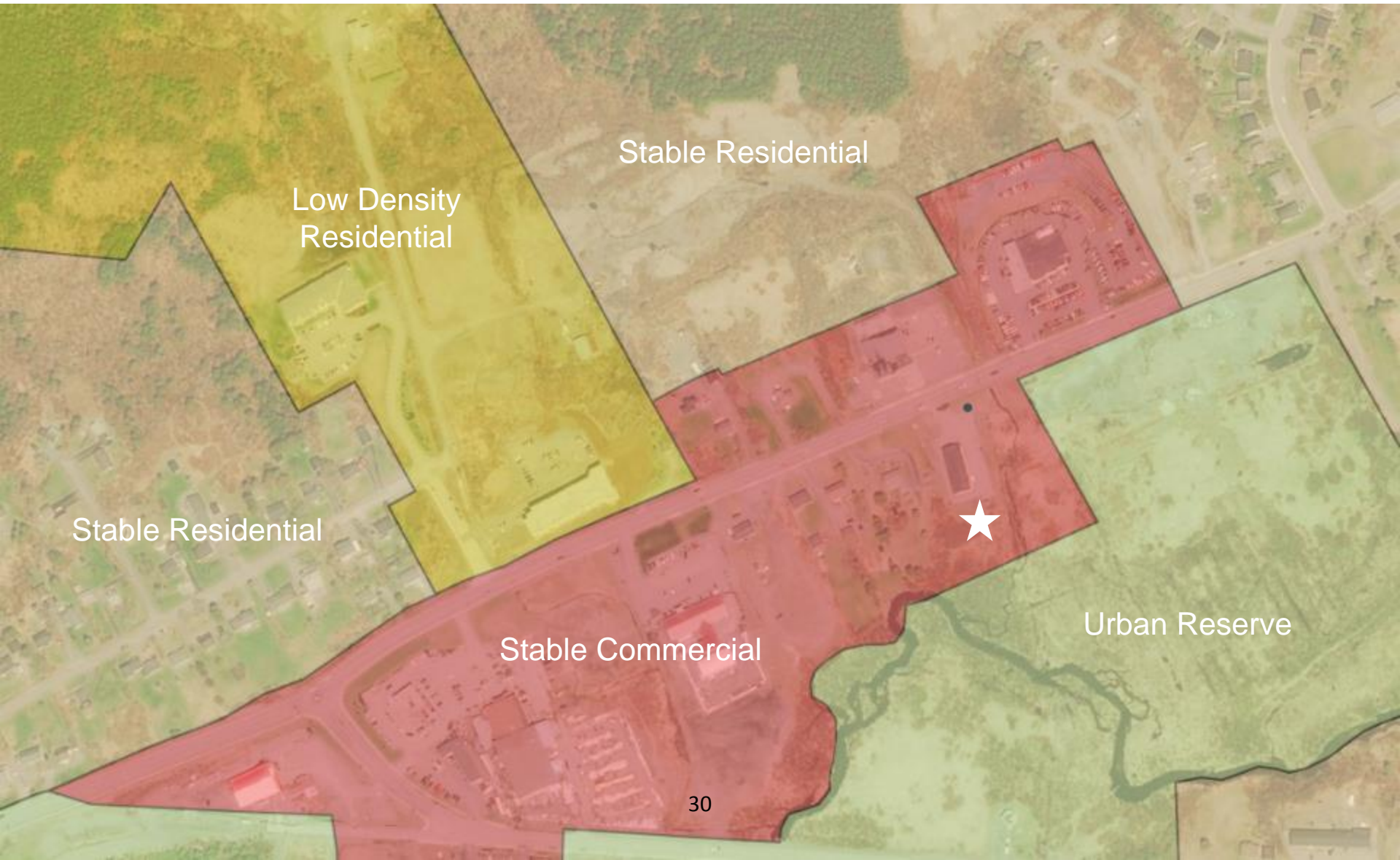


1210 Loch Lomond Road

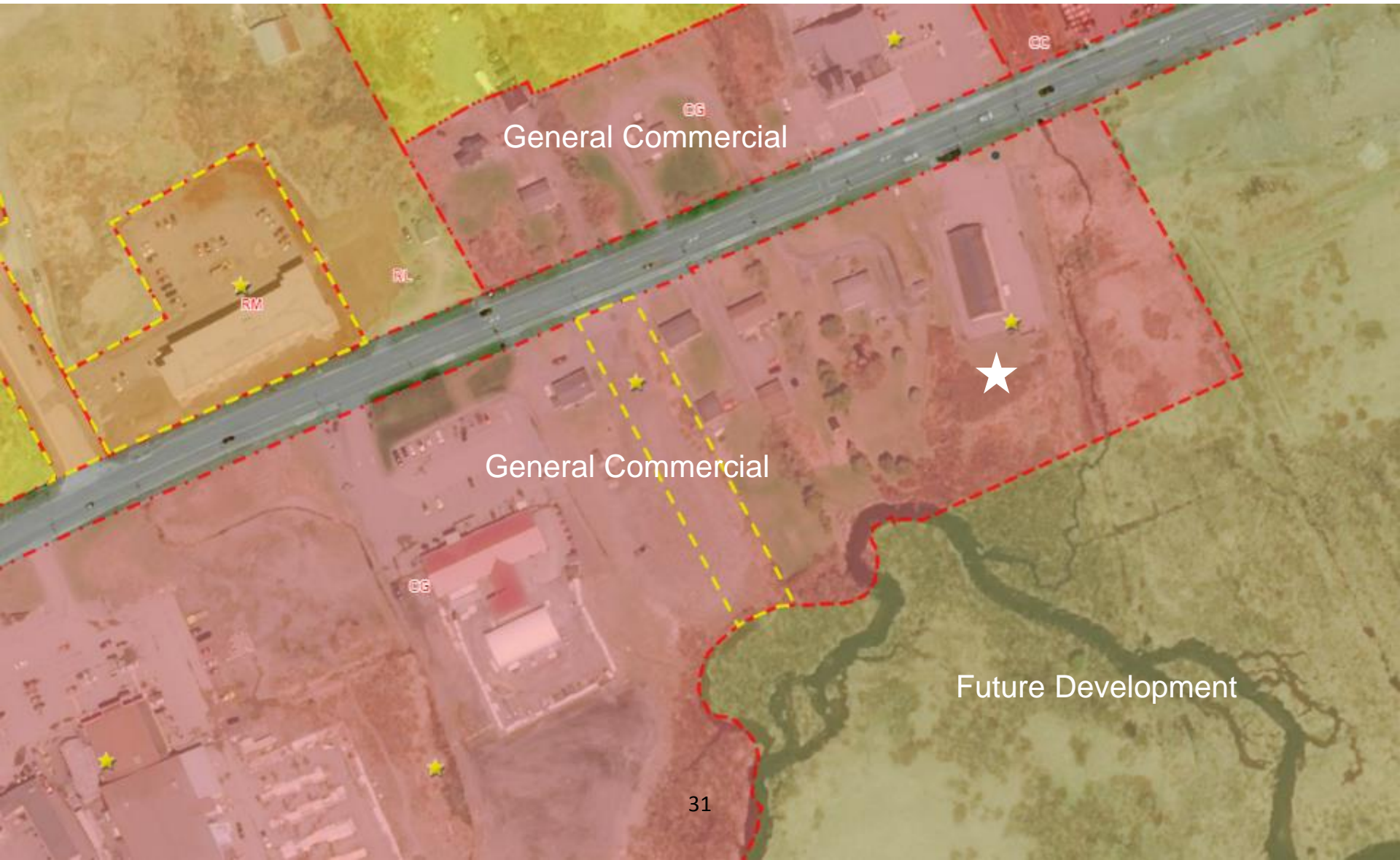
Site & Neighbourhood



Municipal Plan



Zoning



Section 59 Amendment



Development Site

Engagement

On **April 1**, **letters** sent to all property owners within 100 metres of the subject property.

On **March 29**, **public notice** posted on the City's website.

As of **April 8**, **no correspondence** has been received from the public.



Staff Recommendation

That the Planning Advisory Committee recommend to Common Council the approval of the Section 59 Amendment application to permit the construction of the proposed second commercial building and the additional land uses, Commercial Group and Household Contractor Service; and



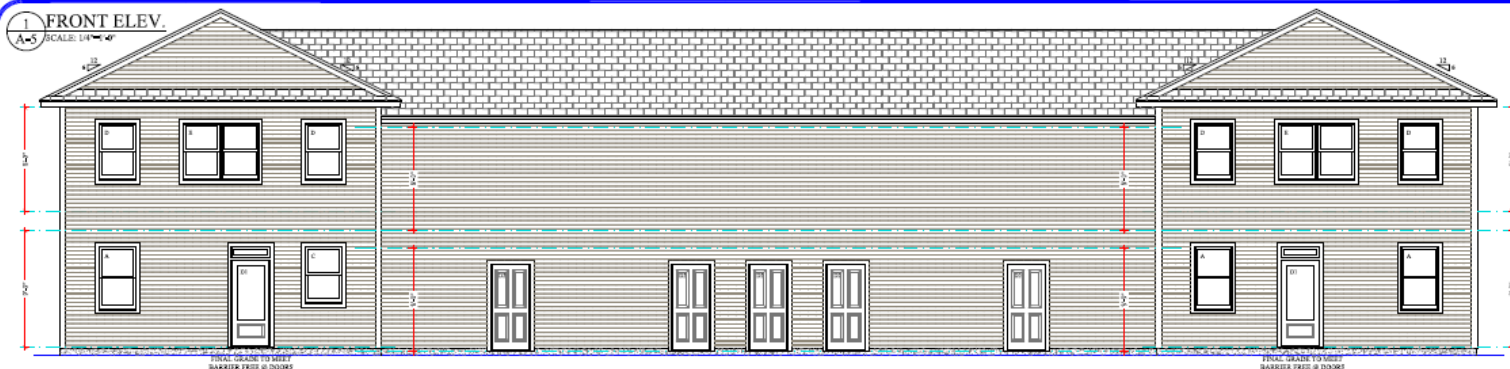
Staff Recommendation

That PAC recommend that the all the previous Section 39 conditions be replaced with Section 59 conditions with modern land use terms and zoning standards, including two new conditions:

- No outdoor storage of materials; and
- A traffic impact statement.



1 FRONT ELEV.
A-5 SCALE: 1/4" = 4'

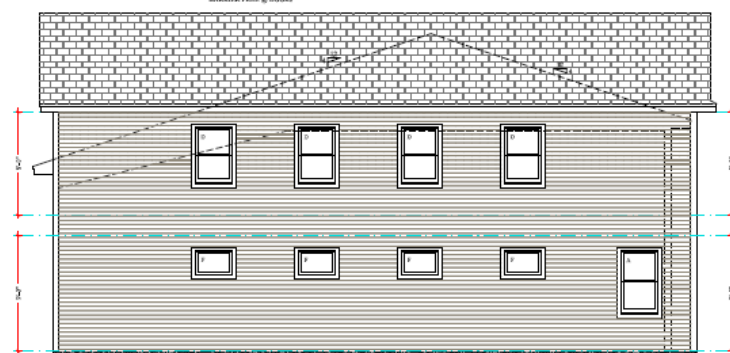


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CURRENT N.E.C. REQUIREMENTS.
ALL WINDOWS AND DOORS TO MEET ENERGY
STAR FOR ENEC OF INSTALLATION.
AIR TIGHTNESS TO BE SEALED AND
MAINTAINED THROUGHOUT ASSEMBLY.
KEY SYSTEM MUST MEET MINIMUM
EQUILIBRIUM (N.E.C.)
FRAME DEFLECTING TO BE INSTALLED IN ALL
MEMBERS.
AIR VAPOR BARRIERS TO BE INSTALLED
ACROSS N.E.C.
RADON MITIGATION SYSTEM TO BE INSTALLED
ACROSS N.E.C.
ALL EXPANDED POLYETHYLENE WITHIN BUILDING
ENVIRONMENT TO BE DESIGNATED WITH 1/2" O.D.
ALL DOORS TO BE INSULATED & DETAIL BY OWNER
TALLANT TO BE APPLIED BETWEEN BOTTOM PLATE
& SUB-FLOOR ALONG PERIMETER.

BRACING NOTES:
ALL CEILING TO BE STRAPPED WITH A MINIMUM
1"x4" S.P.A. TO PROVIDE BOTTOM COMB BRACING.
ALL ROOF TRUS BRACING PROVIDED BY TRUSS
SUPPLIER TO BE INSTALLED AS PER MANUFACTURER
INSTRUCTIONS.
ALL FLOOR SYSTEMS TO HAVE PERMANENT BRACING
BETWEEN JOIST SPACES AT PER MANUFACTURER
INSTRUCTIONS & RECOMMENDATIONS.
TRADITIONAL CLIMBER USED AT FLOOR JOISTS TO
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R20.13 (N.E.C.) REQUIREMENTS.

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& THE NATIONAL BUILDING CODE AND THE STANDARDS
OF THE NATIONALITY OF CONSTRUCTION.
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LOAD BEARING
INTERIOR WALL



2 LEFT ELEV.
A-5 SCALE: 1/4" = 4'



4 RIGHT ELEV.
A-5 SCALE: 1/4" = 4'



3 REAR ELEV.
A-5 SCALE: 1/4" = 4'

3	PRELIMINARY		
REV.	DESCRIPTION	BY	DATE
 <p>Lockhart's Design & Consulting Specializing in Residential & Commercial Designs www.lockhartsdesign.com</p>			
<p>PROJECT MOGUL MANAGEMENT</p>			
<p>TITLE ELEVATIONS</p>			
DESIGNED BY:	PLANNED BY:	REV.	
DRAWN BY:	CHECKED BY:	A-5	
DATE: 10/10/2020	SCALE: 1/4" = 4'	0	



The City of Saint John

Date: April 9, 2021
To: Planning Advisory Committee
From: Growth & Community Services
Meeting: April 13, 2021

SUBJECT

Applicant: Parsi Development NB Limited
Landowner: Parsi Development NB Limited
Location: 300 Sydney Street
PID: 00000034
Plan Designation: Medium to High Density Residential
Existing Zoning: Major Community Facility (CFM)
Proposed Zoning: High-Rise Residential (RH)
Application Type: Rezoning
Jurisdiction: The *Community Planning Act* authorizes the Planning Advisory Committee to give its views to Common Council concerning proposed amendments to the Zoning By-law. Common Council will consider the Committee's recommendation at a public hearing on **Monday, April 19, 2021**.

The *Community Planning Act* authorizes the Planning Advisory Committee to grant reasonable variances from the requirements of the Zoning By-law. The Committee can impose conditions.

EXECUTIVE SUMMARY

The Proposal is to develop a twelve storey, 180-unit multi-residential building at 300 Sydney Street. The application seeks to rezone the 7,881 square metre property, which is currently vacant, from Major Community Facility (CFM) to High-Rise Residential (RH).

Staff have assessed the proposal against the applicable policies of the Municipal and Secondary Plans and are recommending approval of the application for the following reasons:

- The proposed development advances the aspirations of the Municipal and Secondary Plan to drive density to the urban core and waterfront areas to provide high quality and much needed housing supply.
- While the height exceeds the allowable height established in the Central Peninsula Secondary Plan and Zoning By-law, the density as proposed can be accommodated in the proposed location as it is a transitional area without an established built form character.
- Design features are recommended based on a third party design review to ensure a high quality public realm, these include material and colour palettes on the upper stories which mitigates the massing of the building.
- Infrastructure and traffic impacts can be mitigated at the developer's cost.
- Significant public benefits proposed by the developer in exchange for the increased height and density.

Staff recommends approval of the rezoning and amendment to the Maximum Building Height map, with the proposed conditions contained in the Staff recommendation, the Planning Advisory Committee will consider the variance request related to the upper storey step backs, with the Development Officer considering variances related to access, lot area and height/side yard setback. Approval of the step back variance is recommended on account of the variance being reasonable and desirable for the development of the parcel of land and recommended design changes related to the interface with the lower stories with the Public Realm.

RECOMMENDATION

1. That Common Council rezone a parcel of land having an area of approximately 7881 square metres, located at 300 Sydney Street, also identified as PID Number 00000034, from **Major Community Facility (CFM)** to **High-Rise Residential (RH)**.
2. That Common Council, pursuant to the provisions of Section 59 of the *Community Planning Act*, impose the following conditions on the parcel of land having an area of approximately 7881 square metres, located at 300 Sydney Street, also identified as PID Number 00000034:

- (a) The development and use of the parcel of land be in accordance with detailed building elevation and site plans, prepared by the proponent and subject to the approval of the Development Officer, illustrating the design and location of buildings and structures, garbage enclosures, outdoor storage, driveway accesses, vehicle, and bicycle parking, loading areas, landscaping, amenity spaces, signs, exterior lighting, and other such site features;
- (b) Trees and shrubs be planted in the front, flankage, and rear yards, as well as the side yard where possible, to the same standard required by the Zoning By-law for developments in Commercial and Community Facility zones, as per the landscaping plans, and that such trees and shrubs be planted within one year from the issuance date of the development permit;
- (c) All infrastructure improvements to support the development will be completed at the developer's sole responsibility and expense including:
 - i. Extension of the watermain to the development site and possible upsizing of the watermain, should it be required, to provide adequate flows for the proposed development.
 - ii. Extension of the sanitary sewer main to serve the development site, with the provision of a private sewage lift station and force main, should it be required.
 - iii. Provision of recommended transportation network improvements, detailed in the Traffic Impact Statement dated February 17, 2021 prepared by Englobe Limited for the proposed development at 300 Sydney Street.
 - iv. Provision of a concrete sidewalk and concrete curb and gutter along the Sydney Street frontage of the site.
- (d) That the development be constructed above existing grade given the significant tidal influences in this area and not include a basement level below the current elevation of Sydney Street.
- (e) That the elevation plans demonstrate the following elements related to the building design:
 - i. incorporation of design elements from the adjacent Ordinance Building to provide an improved differentiation of the two lower stories and clearer differentiation of the building base.

- ii. improved use of material treatments and colours on the sides of the building which do not face the water to break up the massing of the building;
 - iii. incorporation of design elements for the building entrances to increase their prominence and connection to circulation areas and Public Streets, particularly on the Sydney Street façade;
 - iv. incorporation of design elements to provide additional detailing and articulation on the two lower stories to improve the connection to the public realm and avoid blank walls.
- (f) That the development shall be completed within 5 years of the date the rezoning came into effect. If it is not completed within that time, Council may take steps to cancel the resolution and agreement and repeal the rezoning pursuant to Section 59(5) and 59(6) of the *New Brunswick Community Planning Act*; and,
- 3. That Common Council authorize a Section 131 agreement between the City and the Developer for the following public benefits associated with the development:
 - i. A contribution of \$130,000 to the City towards improvements at Rainbow Park including:
 - 1. An updated Master Plan for Rainbow Park;
 - 2. Repairs of the existing basketball court;
 - 3. Enhanced landscaping; and
 - 4. Funding for the temporary operations of the outdoor ice surface.
 - ii. A contribution of \$10,000 to the City towards a bus shelter to be located along Broad Street in the vicinity of the development;
 - iii. A contribution of \$20,000 to the City towards the beautification of the Tin Can Beach area.
- 4. That Common Council amend Schedule G of the Zoning By-law and increase the maximum building height from 14 metres to 39.3 metres for the parcel of land having an area of approximately 7881 square metres, located at 300 Sydney Street, also identified as PID Number 00000034.
- 5. That, subject to Third Reading being given to the rezoning by Common Council, that the Planning Advisory Committee grant a variance to allow for the building to be developed without the provision of 3 metre façade step backs at elevations of 14 metres and 24 metres.

DECISION HISTORY

No previous decisions have been rendered for the subject property.

ANALYSIS

Proposal

Parsi Development NB Limited has applied to rezone a 7881 square metre parcel of land at the end of Sydney Street to permit the development of a 12 storey (including penthouse), 180-unit multiple dwelling. The two lowermost floors of the building will be internal parking providing 111 parking spaces. The nine floors above this will contain 180 dwelling units with the penthouse level containing common amenity areas including a gym, café, and rooftop courtyard. Additional amenity space is provided through balconies for each unit.

The building will provide 45 3-bedroom units between 1400-1550 square feet, 45 2-bedroom units between 1120-1290 square feet, and 90 1-bedroom units between 790-940 square feet.

A surface parking lot will be located to the rear of the building in the eastern portion of the site with two driveway accesses provided to the development from Sydney Street.

Site and Neighbourhood

The site is a vacant lot located within the City's South End neighbourhood along the east side of Sydney Street. A rail spur is located to the south of the site and the armoury property is located immediately to the east and north of the site. A historic property, the Ordnance Building, is located further to the north of the site along Sydney Street and is currently used as an office building. A City-owned parking lot is located across Sydney Street from the site with a single storey commercial building located north of Vulcan Street.

The surrounding area contains a mix of institutional, commercial, and industrial zoning. Lands immediately north and east of the site are zoned Major Community Facility (CFM). Existing commercial buildings located to the northwest of the site along Sydney Street are zoned Business Park Commercial (CBP). City owned lands located west of Sydney Street, including a parking lot and the former sugar refinery site are zoned Integrated Development (ID) and Waterfront Commercial (CW) respectively. Port lands located further to the south and southeast of the site are zoned Transportation (T).

Municipal Plan and Rezoning

Municipal Plan and Central Peninsula Secondary Plan (CPSP)

Lands to the east of Sydney Street are designated Medium to High Density Residential in the Municipal Plan with the area west of Sydney Street designated as Primary Centre. The Primary Centre area, located northwest of the site, forms the mixed-use core of the Uptown neighbourhood.

The Medium to High Density Residential designation is intended for higher density housing forms such as the development proposed for the site and corresponds with the South End Intensification Area established in Schedule A of the Municipal Plan. Over the horizon of the Plan, 45% of all new development is intended to occur in these urban intensification areas. This direction provides support for the development of the site with a higher density, multiple unit building as proposed by the applicant.

On a broad basis, the Municipal Plan sets a density target of 45 units per net hectare for Medium to High Density Residential Areas. Completion of the proposed building would increase the current density to approximately 35 units per hectare.¹

With regards to the Secondary Plan, the site is located in the South Waterfront area of the South End. Key directions for the South Waterfront include:

- Development of new infill, mixed-use development to feature contemporary architecture that takes advantage of expansive views of the inner harbour.
- Low to mid-rise buildings that step down to the South Waterfront with varying setbacks depending on existing and proposed built form.
- A combination of on-street, surface, and structured parking to accommodate residents, employees, and visitors.
- Improved public realm connections to the South Waterfront, including new pedestrian crossings on Broad Street.
- New large civic buildings that provide education, cultural, and recreation opportunities for residents, employees, and visitors.

Big Moves are also identified over the horizon of the CPSP for catalytic projects that would define key areas of the Central Peninsula. Within the neighbourhood of the proposed development, the Plan identifies development of a new recreation and entertainment destination on the former sugar refinery site as one of these Big Moves. This is envisioned to include new commercial or recreation facilities on the northwest section of the former sugar refinery site, which is the area comprised of the existing parking lot located directly across Sydney Street from the subject site. The proposed Landmark Development and this future commercial or recreational building would serve to frame the lower end of Sydney Street and the corresponding vista along Sydney Street towards Tin Can Beach and the Bay of Fundy. As the Landmark will serve to further establish the built form context in this area to guide future development, its design elements are an important consideration given the immediate surrounding built form context is one largely including low rise development or vacant lands.

A detailed assessment of the proposal with respect to the policies of the Municipal Plan and CPSP is provided in Attachment 2. This was also informed by a Third-Party Design Review of the proposed site layout and building with an emphasis on the building design elements.

¹ Based on analysis conducted in January 2020 for “The Wentworth” application at 95 Wentworth Street.

Staff note that while the height of the building is a departure from the direction established in the CPSP with a 14-metre maximum building height established for this area, compared with the proposed 39.3 metre building height.

Amendment to the Height Map

In preparing the Central Peninsula Secondary Plan the 14-metre height limit was applied as a blanket height guideline outside of the Uptown as a means to assess compatibility and secure public benefits. Staff note a height range of 11 metres to 14 metres for newer multi-unit residential developments is common in the City given the prevalence of smaller scale, medium rise development such as that found in the Mid-Rise Residential zone, the City's more prevalent zone for multiple unit dwellings.

Staff note, the surrounding neighbourhood context provides limited development that is beyond three stories in height, which does not provide an established built form character for the building to respond to in terms of built form and height.

Height is an important component for the developer to provide the required number of units to make the project financially viable and to provide housing that responds to the demands and needs of the community. In this case, the developer has provided a mix of one-, two-, and three-bedroom units that respond to multiple demographics within the market including single tenants, young couples, families requiring multiple bedrooms, and seniors. This approach works towards addressing housing demand currently being experienced in the market and flexibility for future market demands. The introduction of new units into the market also works towards meeting the overall demand for housing affordability.

A key consideration of building height is the impact on the public realm adjacent to the building in terms of the scale and massing of the building with respect to the pedestrian environment and impacts on shadows along Public Streets. In this case, design elements are provided to reduce the effects of massing and height. These elements include differing colours and material types on the upper stories of the building (fibre cement siding and composite metal cladding) and horizontal relief provided through balconies on the facades. The building is also set back from the street with a 6-metre front yard to provide an offset between the face of the building and the public realm.

With respect to the heritage building located to the north of the subject site, Staff note there are design elements recommended for incorporation on the lower two stories of the building that would provide a better connection to the public realm and surrounding context. The impacts of height on adjacent properties to the north are also mitigated by having a 10.8 metre side yard along the northern property boundary as required by the standards of the RH zone. With respect to shadows cast by the building on adjacent streets, Staff note shadow impacts on Sydney Street will be associated with the morning hours and not for the full day.

There are not any anticipated wind tunnel effects given the low height of surrounding buildings, however this may be reviewed in the future in this specific area and other locations within the

Central Peninsula as development occurs. Immediate wind impacts as a result of the development are largely expected to be focused on the proposed building and possibly include building vibration, whistling, low-scale wind vortexes, and snow drifting which should be reviewed by the designer as the building design is finalized.

A third-party design review by EXP was conducted to provide a review of the urban and architectural design of the proposed building and its relationship with the public realm and surroundings. This included consideration of constructive improvements to the design based on the Municipal Plan and Secondary Plan as well as best practice. Staff have considered the feedback submitted from EXP, reviewed the findings with the development team, and in turn, supported the staff recommendation. While some of the recommendations from the third-party review were incorporated into staff's recommendation, others were not, and staff will provide clarity on our rationale in the coming paragraphs.

This design review found that the material and façade design, along with the incorporation of balconies provide an arrangement that attempts to mitigate the scale and massing of the building. In particular, the use of black cladding in the upper stories is useful to bring focus to the lighter areas of the façade minimizing this massing. These elements provide support to the requested variance to not provide façade step backs of 3 metres at the 14 metre and 24 metre elevations of the building. Key issues identified in the design review relate to the overall design of the building which could exist anywhere which does not reference the adjacent Ordinance building from a design perspective and the two lower levels which are essentially blank facades at the street level and do not provide a strong relation to the pedestrian realm.

Opportunities for refinements to the design were identified through the third-party design review including:

- Increased engagement with Sydney Street which could be achieved through design revisions including:
 - Relocating the main entrance lobby to the Sydney Street-facing façade.
 - Improving the pedestrian experience along the Sydney Street corridor, through the provision of small commercial spaces, such as an ice cream shop or a small gallery to support Tin Can Beach and building residents, and prominent windows or amenity spaces to show internal activity at the street levels. Staff note restrictive covenants placed on the development of the site by a previous owner prohibit residential use on the ground or underground floors, which the proponent has responded to by using the two lower levels of the building for internal parking. In addition, the High-Rise Residential zone has a provision for allowing for certain commercial uses as-of-right, provided they are located below the third storey of the building. This could also include increased landscaping and amenity space within the building's front yard.

- Incorporating design elements from the adjacent Ordinance Building such as datums, materials or colour pallets to reinforce a connection to the surrounding context.
- Strengthening of the building base and bringing this portion of the building closer to the street may further reduce the appearance of the overall height of the building and provide an improved connection to the Public realm.
- The use of material treatments and colours to break down the massing of the building is an effective strategy is more effective on the sides of the building which face the water. Additional consideration should be given to improving this approach on the other facades particularly the internal façade which is visible from the Armoury site.

The proponent has considered the effects the building will have on shadows onto adjacent properties and Sydney Street. Information provided by the proponent indicates the bulk of the shadows will fall within the development site given the position of the building relative to the sun.

The third-party review noted the designer should consider wind impacts on the proposed building. Upon review of the submitted feedback, the concerns raised by the third-party review were primarily focused on the proposed development, not adjacent properties. The concerns of a possible wind-tunnel would only become pronounced with other additional development occurring in the area, which is why staff do not believe that a wind study would be required at this time.

Based on Staff's review of the proposal and the input provided through the third-party design review, the proposed building generally conforms to the policies advanced in the Municipal Plan and Secondary Plan, but the potential exists for refinements to the design which will strengthen the connection of the base of the building to the public realm along Sydney Street and the surrounding neighbourhood context. Given this, Staff recommend a Section 59 condition requiring that the elevation plans for the building incorporate elements to include but not limited to the following:

- i. incorporation of design elements from the adjacent Ordinance Building to provide an improved differentiation of the two lower stories and clearer differentiation of the building base.
- ii. improved use of material treatments and colours on the sides of the building, which do not face the water, to break up the massing of the building.
- iii. Incorporation of design elements for the building entrances to increase their prominence and connection to circulation areas and Public Streets, particularly on the Sydney Street facade.
- iv. Incorporation of design elements to provide additional detailing and articulation on the two lower stories to improve the connection to the public realm and avoid blank walls.

Servicing and Traffic

Municipal water is available from Sydney Street via an existing 200 mm diameter cast iron main. Infrastructure Development staff note the proponent's consultant must complete a hydrant flow test prior to finalizing the detailed design of the project and provide these results to Saint John Water. The results of this analysis will identify any required upgrades to the area's water supply infrastructure to support the development.

Sanitary sewer servicing is available on Sydney Street approximately 100 metres north of the subject site. Based on modelling conducted to date by Saint John Water, a private lift station and force main will likely be required to service the development based on the grades of the existing sanitary sewers. The developer may consider extending the existing gravity sewer main for connection; however, it is recommended that the developer's engineering consultant consider servicing by a private lift station and force main.

Storm sewer is not available along this section of Sydney Street. Stormwater from the proposed site is currently flowing overland and draining directly into the Bay of Fundy. Stormwater management of the site will require detailed design by the proponent's engineering consultant and may require an easement across private lands to provide an outlet to the Bay of Fundy.

The applicant has engaged a Professional Engineer to complete a Traffic Impact Study for the proposed development which reviewed traffic operations at the Sydney Street/Broad Street intersection. Key findings of the Traffic Impact Study include:

- The additional vehicle traffic generated by the new development will not create deficiencies at the intersection of Sydney Street and Broad Street. Overall intersection operations are expected to remain acceptable up to 5 years beyond the anticipated full build-out.
- Traffic signals are not warranted at the Sydney Street/Broad Street intersection. Intersection operations are forecasted to operate acceptably under unsignalized control (stop signs on the minor street approaches).
- There are no marked crosswalks at the Broad Street/Sydney Street intersection. In accordance with the TAC Pedestrian Crossing Control Guide, Third Edition, a signed and marked crosswalk with side mounted signs is appropriate at this location with post-development traffic volumes.
- Pedestrian infrastructure along Sydney Street south of Broad Street should be upgraded to improve pedestrian access to the development site. This could include sidewalk reconstruction along the east side of the street or sidewalk/trail upgrades along the west side of the street. The improvements should align with plans to extend the Harbour Passage down Sydney Street to improve access to Tin Can Beach.

With respect to required infrastructure improvements needed to support the development, it would be the developer's responsibility to implement these improvements and it would include infrastructure related to water and sanitary sewer servicing and transportation infrastructure as follows:

- Extension of the watermain to the development site and possible upsizing of the watermain should it be required to provide adequate flows for the proposed development.
- Extension of the sanitary sewer main to serve the development site, with the provision of a private sewage lift station and force main, should it be required.
- Provision of a signed and marked crosswalk on the western Broad Street approach to the intersection with Sydney Street.
- Provision of a concrete sidewalk and concrete curb and gutter along the Sydney Street frontage of the site.

It is noted that based on the developer's proposal, it is understood that the development will be constructed above grade. Due to significant tidal influences in this area, Saint John Water supports an above grade development at this location.

Section 59 condition(s) are recommended related to these infrastructure requirements.

Environmental Issues

The covenants registered on title for the site reference a prohibition on dwelling units at or below grade. This covenant is often imposed on brownfield sites that have a history of past uses associated with petroleum products. Within New Brunswick, the Province administers registries of contaminated and remediated sites and sites which have been licensed for petroleum storage. This information is available via the Service New Brunswick Land Registry. Staff have reviewed the site on the Service New Brunswick data base and the presence of the site was not identified in either of these two registries. Staff also circulated the application to the regional office of the Provincial Department of Environment, and they had no concerns with the proposed development. Any required environmental remediation is a matter of Provincial jurisdiction and the developer's responsibility to mitigate for the proposed development.

Climate Change Impacts

The site is located proximate to the Bay of Fundy which presents concerns with the potential for impacts associated with storm surge and sea level rise. Staff have reviewed information provided in the City's Climate Change Adaptation Plan, prepared by ACAP in 2020, which provided Coastal Hazard Flood Risk Mapping for the South Central Peninsula.

This mapping assessed three scenarios prepared by the Province:

- the current Higher High Water Large Tide (HHWLT) (4.6 m),
- HHWLT+1 m (5.6 m) (the average predicted level of sea level rise by 2100), and

- a 1 in 100-year storm event in 2100 (6.8 m).

Water levels associated with these three scenarios would not impact the proposed development. Staff also note the project incorporates two levels of parking above grade on the building's lower stories which provides additional separation between existing ground levels and habitable floor space.

Archaeological Resources

This site, as with any site along a waterfront or river, has a higher potential for the presence of archaeological resources. These matters are under Provincial jurisdiction through the New Brunswick *Heritage Conservation Act* and are a matter to be dealt with by the developer should any archaeological resources be found on site during the construction of the development.

Affordable Housing

The Municipal Plan notes that affordable housing is a core responsibility of the Government of New Brunswick and the Government of Canada, and that the City will support the provision of affordable housing whenever possible. To further support the development of affordable housing in Saint John, the City is currently completing an affordable housing strategy which is anticipated for completion in early 2022.

Currently within the municipal legislative context, the City's role is one of encouraging the development community to build affordable housing and reducing legislative barriers that exist through regulations such as the Zoning By-law.

In this case, the developer is proposing a project which requires additional building height and variances from the standards of the Zoning By-law. This provides for a building design that provides a sufficient number of units for a financially viable project which responds to housing market needs within the community. The proposed development provides a mix of one-, two-, and three-bedroom units that respond to multiple demographics within the market. Although not incorporating affordable housing units as defined by the Zoning By-law, introduction of new units into the market through the project also works towards meeting the overall demand within the housing market which provides for a degree of affordability through increased supply.

Railway Proximity

A railway spur is located south of the site on lands owned by Canadian National Railway. At one time, this rail spur provided a connection to lands west of the development site with rail tracks still existing on Port lands located west of the subject site. Staff note that the rails have been removed on lands to the west of Sydney Street with the rail line currently terminating at a dead-end east of the potash terminal.

The proximity of new development to railways has been evaluated by the railway industry and municipal associations, with a 2013 document prepared to promote compatible development in areas proximate to rail lines.² This document is meant as a guideline and is not a binding policy or regulation on municipalities or developers and landowners.

The rail line south of the site functions as a branch or spur line, with lower train volumes, and lower speed trains. The guidelines recommend a 15-metre setback between the building and the railway corridor. This setback distance can have both a horizontal and vertical component as illustrated in Figure 1, and serves to provide a buffer from railway operations, dissipate rail-oriented emissions, vibrations, and noise; and accommodate a safety barrier. The building would have a setback of 13.3 metres, comprised of a 9-metre setback between the property line and the proposed building and a 4.3 metre elevation difference from grade at the south wall of the building to the lowest residential floor. This separation is further enhanced by an additional elevation difference of between two to three metres between the rail spur and the site.

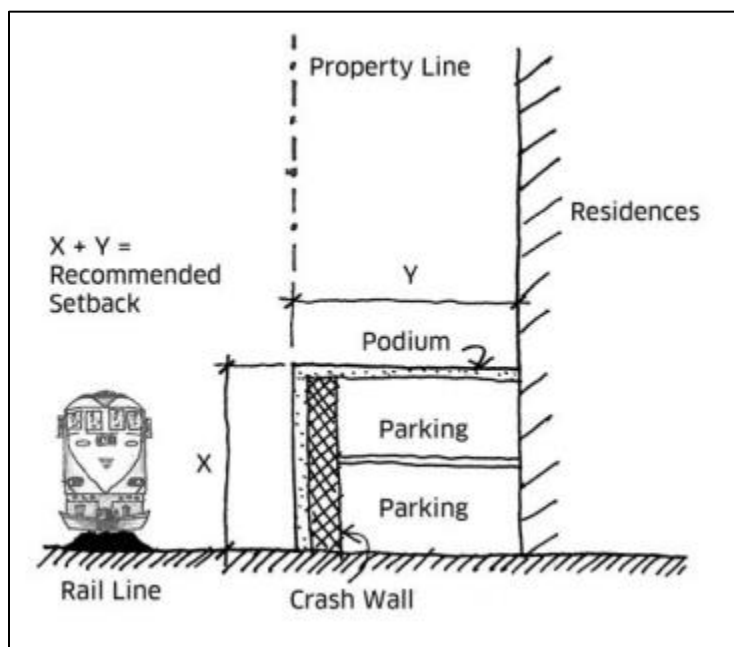


Figure 1. Rail Setback³

Zoning and Variances

The application seeks to rezone the subject property from the Major Community Facility (CFM) Zone to the High-Rise Residential (RH) Zone. The RH zone is consistent with other developments on the Central Peninsula, where taller buildings are zoned High-Rise Residential (RH). The development includes 204 parking spaces which falls within the range required by the zoning by-law for 180 units (144 to 225 spaces based on a possible 80% reduction and 125%

² Guidelines for New Development in Proximity to Railway Operations. Railway Association of Canada / Federation of Canadian Municipalities. 2013.

³ Ibid.

maximum). Staff also note the required amenity space is provided through balconies, outdoor landscaped areas, and internal amenity space available to the residents.

While the standards of the Zoning By-law and specifically the RH zone are mostly met, the proposed building and site design requires four variances from the standards of the Zoning By-law to:

- Allow for the building to not provide a 3-metre façade step back at the 14 metre and 24 metre elevations.
- Allow two driveway accesses for a lot having a frontage of less than 75 metres.
- Increase the maximum height of a building in the RH zone from 30 metres to approximately 39.3 metres.
- Reduce the required lot area from 9135 square metres to 7881 square metres.

The step back variance is considered a variance related to building design and falls within the Committee's jurisdiction. The remaining three variances related to lot area, height and driveway access are within the jurisdiction of the Development Officer.

Planning Advisory Committee Variance

Section	Standard	Variance Requested
15.2(a) (b)	<p>(a) Buildings oriented towards a street that are greater than 14 metres in height shall have a minimum step back at 14 metres for any additional storey(s). The step back shall have a minimum depth of 3 metres away from the street facing front façade.</p> <p>(b) Buildings oriented towards a street that are greater than 24 metres in height shall have a second minimum step back at 24 metres for any additional storey(s). The step back shall have a minimum depth of three metres away from the street facing front façade.</p>	To not provide the required 3 metre façade step back at the 14 metre and 24 metre elevations.
<p><u>Staff Response</u></p> <p>A key finding of the third-party design review was that the building height does not significantly impact the Public Realm, even with the lack of the upper storey setbacks as required by the Central Peninsula Overlay Zone. In addition, the design of the building elevations which incorporate a mix of materials and colours serve to reduce the impact of the height and massing of the building. With respect to the impact on the Public Realm along Sydney Street, a more prominent concern is the design of the lower two stories of the building. A series of design considerations to improve the relationship between the two lower floors of the building and Sydney Street and the surrounding neighbourhood context. Staff recommend approval of the variance based on the analysis presented in this report and the recommended Section 59 conditions.</p>		

The applicant notes the articulation of the building provides a visual break in aesthetic and serves to minimize the height and massing consistent with the intent of the upper storey façade setbacks. The applicant also notes it is difficult to provide a structural break every few levels as the constructability and economics of the project are heavily dependent on structural simplicity.

Development Officer Variances

Section	Standard	Variance Requested
10.3(2) (a)	Minimum Lot Area: 9135 square metres (as per number and type of units)	To reduce the minimum lot size to 7881 square metres to recognize existing site conditions.

Staff Response

Minimum Lot Area requirements are a means of regulating density on a site-by-site basis. On a per-site basis, the proposal has a density of 228 units per hectare which meets the minimum density threshold established in the Municipal Plan and will serve to increase the overall neighbourhood density in the south end.

A second test for the reasonableness of a lot area variance is the ability of the site to accommodate the proposed development while still meeting all other requirements of Zoning By-law including parking, amenity space and setbacks from the site property lines. In this case, these standards are met except for the southern side yard. Given the ability of the site to accommodate the proposed development in accordance with the standards of the Zoning by-law, combined with the contribution towards the overall neighbourhood density target established in the Municipal Plan, the Development Officer considers there to be reasonable grounds to grant the variance.

Section	Standard	Variance Requested
10.3(2) (a)	Building height above 30 metres and required side yard setbacks	To increase the height to 39.3 metres while providing a 9-metre side yard setback

Staff Response

The proposed building will have a height of 39.3 metres measured from average grade. The RH zone provides for a maximum height of 30 metres with the provision for an additional 5 metres of building height for each one metre addition to both required side yards. A 30-metre height requires a 9-metre side yard, with a 39.3 metre height requiring side yards of 10.85 metres. The proponent's design provides a side yard setback of 10.87 metres on the north side yard and a 9-metre side yard along the southern boundary of the site.

Staff note the provision of the required side yard at the north of the site responds to the intent of the by-law to mitigate the impact of height on adjacent properties, namely the Armoury property immediately adjacent to the site and the Ordnance building located further to the north. A rail spur is located immediately adjacent to the site with the Potash Terminal and Bay of Fundy located further to the south. Given the context of development to the south of the subject site, the building height and 1.85 metre reduction in the side yard would not have a

significant impact on adjacent lands due the reduced mitigation associated with the reduced side yard south of the subject site.

Staff also note a main factor influencing height of the project, is the incorporation of a two-level internal parkade at the base of the building. This parkade is constructed on the site's existing grade, without excavation below the surface. This adds an additional 6.7 metres to the height of the building, representing most of the amount in which the height exceeds the by-law standard. Staff note that the provision of internal parking in new developments is a major focus of the urban design policies established in the Municipal Plan. Staff also note the site is in proximity to the Bay of Fundy which presents the possibility for a high-water table which would affect the ability to construct parking at an elevation below existing grade. This internal parking area, and the design of the development site which provides surface parking that minimally impacts the view from Sydney Street and the surrounding neighbourhood, conforms to the policy direction of the City's planning documents.

Given this, the Development Officer considers there are reasonable grounds to grant the variance.

Section	Standard	Variance Requested
4.1(2) b)	The number of two-way accesses shall be limited to one for each lot frontage except when the lot has a frontage over 75 metres then two accesses are permitted if a minimum of 20 metres is provided between the centreline of each access measured at the street line.	To provide two accesses on a lot having a frontage of 65.8 metres.
<p><u>Staff Response</u></p> <p>A spacing of 46.5 metres will be provided between the two accesses, which exceeds the 20-metre requirement for spacing between driveways. Given the distance provided between the two driveways, and the relatively large lot frontage, staff consider the variance request reasonable.</p>		

Economic Benefit

The construction of this proposed development will provide economic benefits to the City of Saint John both during and after construction. Based upon financial information provided by the applicant, the construction costs for the building are estimated to be \$36 million. The two-year construction period will spend approximately \$18 million on labour costs and will create 110 jobs.

Once the building has been completed, it is estimated that it will provide \$660,000 in annual property taxes, which will contribute to the City's goal of increasing the City's tax base through new development. The building will require staff to keep the building operational. It is anticipated that there will be 1-2 full time caretakers and concierge for the building and 3-4 part-time cleaners and yard maintenance staff.

Public Benefit

In exchange for additional flexibility on building design components such as height or density, it is common in other jurisdictions to secure public benefit through "Density Bonusing." Municipalities in New Brunswick can create a Density Bonusing By-law in accordance with the

Community Planning Act, which is recognized in Policy BD-6 to be a deliverable of the Secondary Plan. Public Benefits can also be undertaken through Section 59 conditions, imposed by Council as part of the rezoning process or where the developer proposes to establish conditions for a development that are additional to those required by a zoning by-law in accordance with Section 131 of the *Act*.

Public benefits are not prescriptive but offered to the Proponent through a negotiation. Through discussions with the developer, City Staff have focused on improvements to public spaces in the south end in the vicinity of the proposed development. In exchange for the provision of additional building height and a variance from the standards requiring façade step back at the upper stories of the building, the Developer has agreed to a package of public benefits having a benefit of approximately \$160,000 largely focused on Rainbow Park. Specific components of this public benefit package are:

- A contribution of \$130,000 to the City to fund improvements to Rainbow Park including:
 - Development of an updated Rainbow Park Master Plan, which is an Action Item in the CPSP.
 - Basketball Court Repair – The basketball court in Rainbow Park requires repair and potential expansion. The developer will contribute towards this undertaking.
 - Rainbow Park Landscaping – The need for additional landscaping in Rainbow Park along the interfaces with the Public Street right-of-ways has been identified in the CPSP as an improvement to the park and surrounding Public Realm. The developer's contribution will assist the City in making these improvements.
 - Outdoor Ice Surface Operation – Rainbow Park features an outdoor ice surface that has not operated in the past few years. Annual operating costs are approximately \$ 10,000. The developer has agreed to provide funds to the City in order to operate the outdoor ice surface for a set period of time.
- Broad Street Bus Shelter – The proposed development will introduce additional residents in the south end, potentially increasing transit ridership on routes serving the area. Given this, a transit shelter along the Broad Street was identified as a potential public benefit. The developer has agreed to provide \$10,000 towards this item.
- Tin Can Beach Beautification – The development site is proximate to Tin Can Beach, which has become a community amenity providing access to the water. Although the beach area is not owned by the City, the potential exists for additional improvements to the city-owned right-of-way at the southern end of Sydney Street to provide additional public amenity and further beautify this area. The developer has pledged \$20,000

towards this initiative. Staff note potential improvements include additional street furniture and landscaping in this area.

The Staff Recommendation includes formalizing this approach through an agreement with the Developer.

Conclusion

Staff recommend approval of the application as it conforms to the overall intent of the Municipal Plan and Secondary Plan and provides density in a key intensification area while providing a building design that minimizes the impacts of additional height.

Given the analysis provided in this report, the rezoning is recommended along with an amendment to the Maximum Height map. Urban Design and other considerations have been assessed through a Third-Party Review and incorporated into conditions of the rezoning.

ALTERNATIVES AND OTHER CONSIDERATIONS

Based on the Jurisdiction section of the report, the Committee has the authority to formulate alternatives to staff's recommendation or impose conditions.

ENGAGEMENT

Proponent

The proponent has engaged the broader community through a project website launched in late-March 2021. A virtual Town Hall was held by the proponent on April 8, 2021.

Public

In accordance with the Committee's Rules of Procedure, notification of the proposal was sent to landowners within 100 metres of the subject property on April 1, 2021. The rezoning was posted on the City of Saint John website on March 29, 2021.

APPROVALS AND CONTACT

Author	Manager	Commissioner
Mark Reade , P.Eng., MCIP, RPP	Jennifer Kirchner , MCIP, RPP	Jacqueline Hamilton , MCIP, RPP

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Email: Mark.Reade@saintjohn.ca
Application: 21-0048

APPENDIX

Map 1: **Aerial Photography**

Map 2: **Future Land Use**

Map 3: **Zoning**

Attachment 1: **Site Photography**

Attachment 2: **Municipal Plan and Secondary Plan Policy Review**

Attachment 3: **Third Party Design Review**

Submission 1: **Site Plan**

Submission 2: **Floor Plans**

Submission 3: **Elevation Plan**

Submission 4: **Renderings**

Submission 5: **Traffic Impact Statement**

CPSP Policy	300 Sydney Street Assessment
SE-3 Position Sydney and Charlotte Streets as key entryways to the South Waterfront	A Section 59 condition is proposed for additional sidewalks to be provided along Sydney Street.
BD-2 Protect public views to the water along key public streets and open spaces.	The setback of the proposed building does not impede the view to the water along Sydney Street.
BD-4 Consider proposals for development that exceed the Zoning Bylaw provisions for height on the Central Peninsula through an amendment to the Zoning Bylaw. In evaluating such proposals consideration shall be given to the following criteria: <ul style="list-style-type: none"> • The proposed height variance does not negatively impact pedestrian street conditions and sun exposure and wind impacts are considered through the design. • That the proposal does not adversely impact the character of the City's Heritage Conservation areas; • The general intent and purpose of Plan SJ, this Secondary Plan, and supporting Bylaws is maintained. 	<p>Although the RH zoning being sought for the development allows for the building height of 39.3 metres if additional side yard setbacks are provided, the Height Map for the Central Peninsula, limits building height on this site to 14 metres. This 14-metre height is guided by the intent of the CPSP to have buildings "step down" in height to the waterfront.</p> <p>The subject site is not located within a Heritage Conservation Area, the Brunswicker / Ordnance Corps Heritage Conservation Area (The Ordnance Building) is located approximately 20 metres north of the site and provides a built-form reference for the proposed building. Design elements from this building could be incorporated into the design of the proposed building to improve the relation of the lower stories of the building to the surrounding public realm.</p> <p>A third-party review of the proposed design noted the use of materials and colours on the upper stories of the building mitigates the massing of the building and would serve to reduce the appearance of the overall building height.</p>
BD-5 Direct new tall buildings to areas where height currently exists, where height can be accommodated seamlessly, or where height is insulated from existing neighbourhoods due to topography or other site conditions.	<p>The height of the proposed building exceeds that of the surrounding area, with the Armoury and Ordnance Building being the tallest buildings in the area. The height of the proposed building is insulated from existing neighbourhoods given the limited development in the immediate area.</p> <p>A third-party review of the proposed design noted the use of materials and colours on the upper stories of the building mitigates the massing of the</p>

	<p>building and would serve to reduce the appearance of the overall building height.</p> <p>The proponent is proposing a series of public benefits to provide an offset for the increased height of the building, these benefits are largely focused on key action items from the CPSP.</p>
<p>BD-7 Ensure the massing of buildings considers and protects the pedestrian street level experience, including sky views and sunlight penetration to the street while minimizing wind.</p>	<p>The lower-level elevations of the building façade is a particular concern from a design perspective. While the use of the ground floor is governed by restrictive covenants from a past landowner. The current design appears to incorporate a black blank wall along the ground floor based on the renderings with the elevations referencing grey cementitious panels on this portion of the façade.</p> <p>While a key intent from an urban design perspective is active ground floor uses, a level of animation in a ground floor façade can provide a degree of relief from a blank wall and aid in minimizing the visual impact of the scale of the building relative to the adjacent context. The provision of landscaping, site amenities such as walkways and benches and a more prominent building entrance can improve the building's relation to the street and Public realm. The third-party review highlighted alternatives that could be incorporated into the building to improve the relation of the lower stories to Sydney Street and the surrounding neighbourhood context.</p>
<p>BD-8 Provide transitions in scale to Heritage Conservation Areas, low-rise residential areas, and the waterfront.</p>	<p>The design review noted the use of the colour black on the upper stories had the effect of mitigating the height of the proposed building in relation to the existing context. A large side yard is provided on the north portion of the site to mitigate the height impacts on the Ordnance building.</p>
<p>BD-11 Outside of the Trinity Royal Heritage Conservation Area, ensure street wall heights maintain a comfortable, human scaled street enclosure, generally a maximum of five storeys in commercial areas and two storeys in residential neighbourhoods.</p>	<p>A front yard setback is provided to minimize the impact of the street wall and the provision of additional landscaping and site amenities within this front yard setback would aid in minimizing the impact of the building height and street wall.</p>

Municipal Plan Policy Review

<p>Policy LU-31.1 Recognize that in the future there may be an opportunity to redevelop the lands South of Broad Street, including the Barrack Green Armoury property to accommodate more mixed-use, urban forms of development.</p>	<p>Although not a mixed-use development, the proposed high-density residential building form represents an urban form of development.</p>
<p>Policy LU-45 Within the Medium to High Density designation, permit a range of housing types with an emphasis on the provision of higher density housing forms such as apartment, condominium, and townhouse units. Nothing in the Municipal Plan, however, shall be interpreted to prohibit lower density forms of residential development in this designation, provided that minimum residential density targets have been met for the Intensification Area as a whole. Other compatible uses may be permitted in the Medium to High Density designation without amendment to the Municipal Plan including, but not limited to convenience stores, home occupations, neighbourhood retail uses, parks, and community facilities</p>	<p>The proposed multiple unit development conforms to this policy.</p>
<p>Policy LU-46 Achieve a minimum gross residential density per hectare in lands designated Medium to High Density of 45 units per net hectare. Density shall not be calculated on a property by property basis but shall be calculated in the Intensification Area as a whole.</p>	<p>The density of the proposed development will support this policy and increase the overall neighbourhood density towards the target established in the Municipal Plan.</p>
<p>Policy UD-6 Consider the following guidelines for the development and enhancement of the public realm in the Uptown Waterfront:</p> <p>c. Reinforce and emphasize important pedestrian connections from the Uptown Waterfront to key streets in the Trinity Royal Heritage Conservation Area, in particular along King, Princess, Duke, Charlotte, and Sydney Streets;</p>	<p>c. Sidewalk and crosswalk upgrades are to be provided in accordance with the recommended Section 59 conditions. These will improve pedestrian connections in accordance with the direction of the Municipal Plan.</p> <p>e, f, and q. The provision of landscaping, site amenities such as walkways and benches and a more prominent building entrance can improve the building's relation to the street and Public realm. In addition Public Benefits proposed by the developer improve landscaping of parks and the Public Realm.</p>

<p>e. Use the physical interface between sites in the Waterfront, and between the Uptown Waterfront and the Uptown to reinforce interesting and animated edges through pedestrian connections, landscaping, new buildings, and public places that collectively create interesting and inviting places;</p> <p>f. The quality and character of public spaces, landscapes and buildings in the Uptown Waterfront are essential to creating a memorable experience. Design buildings and open space projects to a very high standard with the use of quality materials and site furnishings to achieve durability and minimize maintenance costs, while also characterizing the Waterfront as an evocative and unique place;</p> <p>q. Demonstrate a well-articulated building façade with a regular pattern of windows and a highly transparent ground floor façade on both the street and waterfront edges for all waterfront development.</p>	
<p>Policy UD-9 Ensure all development proposals generally conform to the following General Urban Design Principles:</p> <p>a. That new development respect and reinforce the existing and planned context in which it is located through appropriate setbacks, landscaping, buildings entrances, building massing, architectural style and building materials. Specifically, the built form of new development shall be designed to achieve the following objectives for specific areas of the City:</p> <p>i. In the Primary Centre and Neighbourhood Intensification Areas, as identified on the City Structure map (Schedule A), new development will be located and organized to frame and support the surrounding public realm and massed to fit harmoniously into the surrounding environment, including</p>	<p>a. The proposed building setbacks meet the standards required in the Zoning By-Law. As the building is not located directly on the waterfront, it should not impact the public accessibility to the space. Rather, added sidewalks and potential lighting improvements would increase safety for waterfront users.</p> <p>Revisions to the proposed building and site design, including those which focus on the lower-level facades and improve the relationship between the building and Sydney Street are recommended,</p> <p>b. The building entrance is located within the centre of the site rather than along this street. As this building is a private residence, this criterion is met, however the prominence of both the front and rear pedestrian entrances can be increased through a revised design.</p>

<p>appropriate transitions in height and massing to areas of lower intensity development, as set out in Policy UD-11.</p> <p>iii. Special considerations are established for the Uptown Waterfront as set out in Policy UD-13 to promote the development of a vibrant mixed-use, and publicly accessible waterfront;</p> <p>b. Locating building entrances facing the public street;</p> <p>c. Designing sites to incorporate existing natural features and topography;</p> <p>d. Designing sites to protect, create and/or enhance important view corridors to the water or landmark sites or buildings;</p> <p>h. Using quality, durable building materials and a consistent level of design and detail for all elements of the building;</p> <p>i. Designing for visual interest by incorporating well-articulated building façades, landscaping, local history, public art and/or culture into sites and buildings;</p> <p>j. Directing high-rise buildings to appropriate areas and ensuring their design is sensitive to the neighbourhood and/or heritage context;</p> <p>k. Encouraging sustainability in design by:</p> <ul style="list-style-type: none"> i. Utilizing reused, recycled, renewable or local building materials where possible; ii. Using green building or neighbourhood standards; 	<p>c. The current site does not have any notable features the building design responds to constraints from restrictive covenants on the site.</p> <p>d. The view corridor along Sydney Street will be preserved.</p> <p>h. Revised elevation drawings provided by the applicant detail a mix of fibre cement siding and composite metal cladding. These materials are mixed over various portions of the building facades to minimize the massing of the building.</p> <p>i. The lack of active use on the main level provides a barrier to establishing a visually interesting building. A front yard setback is provided to minimize the impact of the street wall and the provision of additional landscaping and site amenities within this front yard setback would aid in minimizing the impact of the building height and street wall along with additional detailing of the lower stories of the building.</p> <p>j. The plans for the area called for a smaller building. To further connect the building with the surrounding design context, elements of the Ordnance Building, located further to the north along Sydney Street could be incorporated into the design of the lower levels of the proposed building to improve the relationship between the street and the proposed building.</p> <p>k. Staff note the orientation of the building provides for solar gain and stormwater management requirements will result in post-development stormwater flows being limited to the pre-development flow from the site. The applicant has also noted plans for car sharing by residents of the building to reduce reliance on private automobiles.</p> <p>l. A complete CPTED Analysis could be conducted once more detailed plans are created (e.g., full detailed design plans) but not required as a condition of approval.</p>
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<p>iii. Designing for energy efficiency and alternative sources of energy;</p> <p>iv. Designing for water conservation and on-site stormwater management;</p> <p>vi. Designing sites and buildings to work with, rather than against, the natural environment by designing according to the topography, hydrology, ecology, and natural drainage patterns of the site and taking advantage of passive solar gain and natural light; and</p> <p>vii. Using native vegetation for landscaping where appropriate.</p> <p>l. Designing sites and buildings according to the Crime Prevention through Environment Design (CPTED) principles to promote safety and security, in balance with other urban design goals; and</p> <p>m. Locating and screening parking and loading facilities so they are generally not visible from the street, particularly in Centres and Neighbourhood Intensification Areas;</p> <p>n. Limit surface parking between the front of a building and the public street or sidewalk;</p> <p>o. Design safe and direct access to buildings for pedestrians, cyclists, and transit users by providing walkways from the public street, transit stops, and parking areas to main building entrances and including bike parking and end-of-trip facilities, where appropriate, and mid-block connections where possible;</p> <p>p. Design sites and building accesses that are barrier-free, convenient and have clear signage; and</p>	<p>m, n & q. The surfacing parking is largely buffered by the location of the building meeting the direction established in the Municipal Plan.</p> <p>o & p. Detailed design of the building should address increased prominence of the building's pedestrian entrances and additional design features to mitigate the relationship of the lower levels of the building to the public realm.</p>
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<p>q. Generally locating surface parking, outdoor storage, loading and other service areas at the rear or side of the property and buffering or screening these functions from adjacent properties and the public realm.</p>	
<p>Policy UD-11 Ensure that new development and significant redevelopment in Neighbourhood Intensification Areas and Primary Centres will be designed to enhance the surrounding public realm and to complement the existing context while providing opportunities for intensification, where appropriate. Development will demonstrate due consideration to:</p> <p>a. Designing sites and buildings for people as the primary focus and with setbacks that are generally consistent with those of adjacent buildings;</p> <p>b. Creating animated, active streetscapes with interesting façades and human scale buildings and setbacks, particularly at the street level. Within the Uptown and other Urban Neighbourhood Intensification Areas, development should generally establish a human scale street wall with an appropriate ratio between the street wall height of the building and the width of the street;</p> <p>c. Where appropriate, ensure heritage streetscapes and Heritage Conservation Areas are reinforced with compatibly scaled and designed development;</p> <p>d. Providing active ground floor uses and avoiding blank façades. Along commercial streets in the Uptown Primary Centre in particular, commercial uses shall be strongly encouraged at the ground floor of buildings with a high degree of transparency at grade to animate the public realm;</p>	<p>a. The proposed design aligns with this requirement through meeting the by-law.</p> <p>b & d. See previous comments regarding street level activation.</p> <p>f. As discussed previously, there is an issue with the scale and transition of this building in relationship to the surrounding areas. The use of varying materials and colour as proposed has the effect of minimizing the massing of the building and additional design approaches can be taken such as the use of the colour black on upper stories (to provide the effect of those areas falling away versus the use of the colour at grade). Additional design elements that could be incorporated to further reduce the prominence of the scale of the building include incorporating design elements that relate to the Brunswicker building and general scale of neighbourhood, specifically on the buildings two lower stories.</p> <p>h. Additional articulation should be provided at the pedestrian entrances of the building to increase their prominence in relation to the overall façade.</p> <p>j & m. A more detailed landscaping design should provide these considerations for onsite users. Incorporate comments from the design review. Possible Section 59 condition.</p> <p>k. Potential improvements include a covered bus stop on Broad Street, and creating a Sydney Street sidewalk. The applicant has committed funds for the construction of a covered bus stop on Broad Street. A Sydney Street Sidewalk will be a condition of the proposed development.</p>

<p>e. Strongly encourage new development to provide ground floor ceiling heights that are consistent with the ground floor ceiling heights of adjacent buildings;</p> <p>f. Creating appropriate transitions in scale and height to areas of lower intensity;</p> <p>g. Defining appropriate standards for above grade step-backs and separation distances of buildings to ensure adequate street level conditions with respect to minimizing wind and maximizing sun penetration and sky exposure;</p> <p>h. Generally locating building entrances to connect directly to the public street network and clearly articulating the building entrance;</p> <p>i. Designing sites and buildings that are barrier-free, convenient and have clear signage;</p> <p>j. Promoting pedestrian comfort with appropriate landscaping, furniture, weather protection and buffers from vehicular traffic;</p> <p>k. Designing for active and alternative modes of transportation by providing convenient access to buildings from transit stops, including bicycle parking and end-of-trip facilities where appropriate, and mid-block pedestrian connections where possible;</p> <p>l. Designing sites and buildings to facilitate social interaction by including patios, courtyards, plazas, and sidewalk amenity space wherever possible to enliven the public or semi-public realm;</p>	<p>l. Suggested enhancements at the front and rear of the building to provide amenity space that benefits from water views and provides activation of the area.</p>
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<p>Policy UD-12 Ensure that in Neighbourhood Intensification Areas and Primary Centres, new development and significant redevelopment will locate and organize vehicle parking, access, and service areas to minimize their impact on surrounding properties and the public realm by:</p> <ul style="list-style-type: none"> a. Sharing services, including public and private driveways, parking, and service areas wherever possible and where zoning permits. b. Sharing and minimizing the width of driveways and curb cuts across sidewalks; c. Providing vehicle service areas within buildings where possible; d. Providing underground parking where possible; e. Generally locating surface parking to the side or rear of buildings; f. Generally locating surface parking, outdoor storage, loading and other service areas at the rear or side of the property and buffering or screening these functions from adjacent properties and the public realm; g. Integrating service connections, vents, mechanical rooms, and equipment within the architectural treatment of the building where possible; and h. Ensuring that parking areas, lobbies, service areas and stairwells are well-lit and visible from other locations, and clearly signed if they are not visible from the public street. 	<p>The design meets this condition, largely through providing internal parking and locating the parking to the rear of the building.</p>

<p>Policy HS-2 Support the development of a wide range of housing types and forms of tenure in Neighbourhood Intensification Areas to redevelop and revitalize these neighbourhoods, in accordance with good land use planning principles.</p>	<p>The proposed building provides a mix of unit sizes to bolster the rental housing market in the South End Intensification Area.</p>
<p>Policy I-2 In considering amendments to the Zoning Bylaw or the imposition of terms and conditions, in addition to all other criteria set out in the various policies of the Municipal Plan, have regard for the following:</p> <ul style="list-style-type: none"> a. The proposal is in conformity with the goals, policies and intent of the Municipal Plan and the requirements of all City bylaws; b. The proposal is not premature or inappropriate by reason of: <ul style="list-style-type: none"> i. Financial inability of the City to absorb costs related to development and ensure efficient delivery of services, as determined through Policy I-7 and I-8; ii. The adequacy of central wastewater or water services and storm drainage measures; iii. Adequacy or proximity of school, recreation or other community facilities; iv. Adequacy of road networks leading to or adjacent to the development; and v. Potential for negative impacts to designated heritage buildings or areas. c. Appropriate controls are placed on any proposed development where necessary to reduce any conflict with adjacent land uses by reason of: <ul style="list-style-type: none"> i. Type of use; ii. Height, bulk or appearance and lot coverage of any proposed building; 	<p>These are addressed through proposed Section 59 conditions.</p>

<ul style="list-style-type: none"> iii. Traffic generation, vehicular, pedestrian, bicycle or transit access to and from the site; iv. Parking; v. Open storage; vi. Signs; and vii. Any other relevant matter of urban planning. d. The proposed site is suitable in terms of steepness of grade, soil and geological conditions, locations of watercourses, wetlands and susceptibility of flooding as well as any other relevant environmental consideration; e. The proposal satisfies the terms and conditions of Policy I-5 related to timeframes and phasing of development; and f. The proposal meets all necessary public health and safety considerations. 	
<p>Policy I-5 Establish timeframes on the phasing and/or completion of development where it is determined that an application to rezone land is speculative in nature. In this case, Council may require that the rezoning be subject to a Development Agreement which may include provisions including, but not limited to the following:</p> <ul style="list-style-type: none"> a. That, in addition to any other securities or bonds Council considers appropriate under section 39(8) of the New Brunswick Community Planning Act, the applicant provide a certified cheque in the amount of \$1000 to cover expenses relating to the cancellation of the agreement and/or repeal of the rezoning. The \$1000 security shall be repayable on completion of the development for which the rezoning is granted; b. Should construction of the development (or in the case of a phased development, the first phase of the development) not be completed within five (5) years from the date the rezoning comes into effect; 	<p>A Section 59 condition is recommended regarding a five-year time limit.</p>

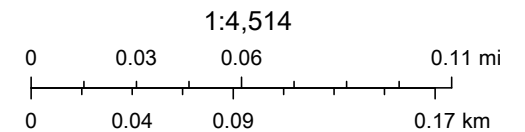
<ul style="list-style-type: none">i. No further development shall be permitted;ii. Council shall immediately take steps to cancel the agreement and repeal the rezoning pursuant to section 39(5) and 39(6) of the New Brunswick Community Planning Act;c. That upon repeal of the rezoning pursuant to section 39(5) and 39(6) of the New Brunswick Community Planning Act, the land to which the agreement pertainsd. shall revert to its last previous zoning designation.	
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Air Photo - 300 Sydney Street



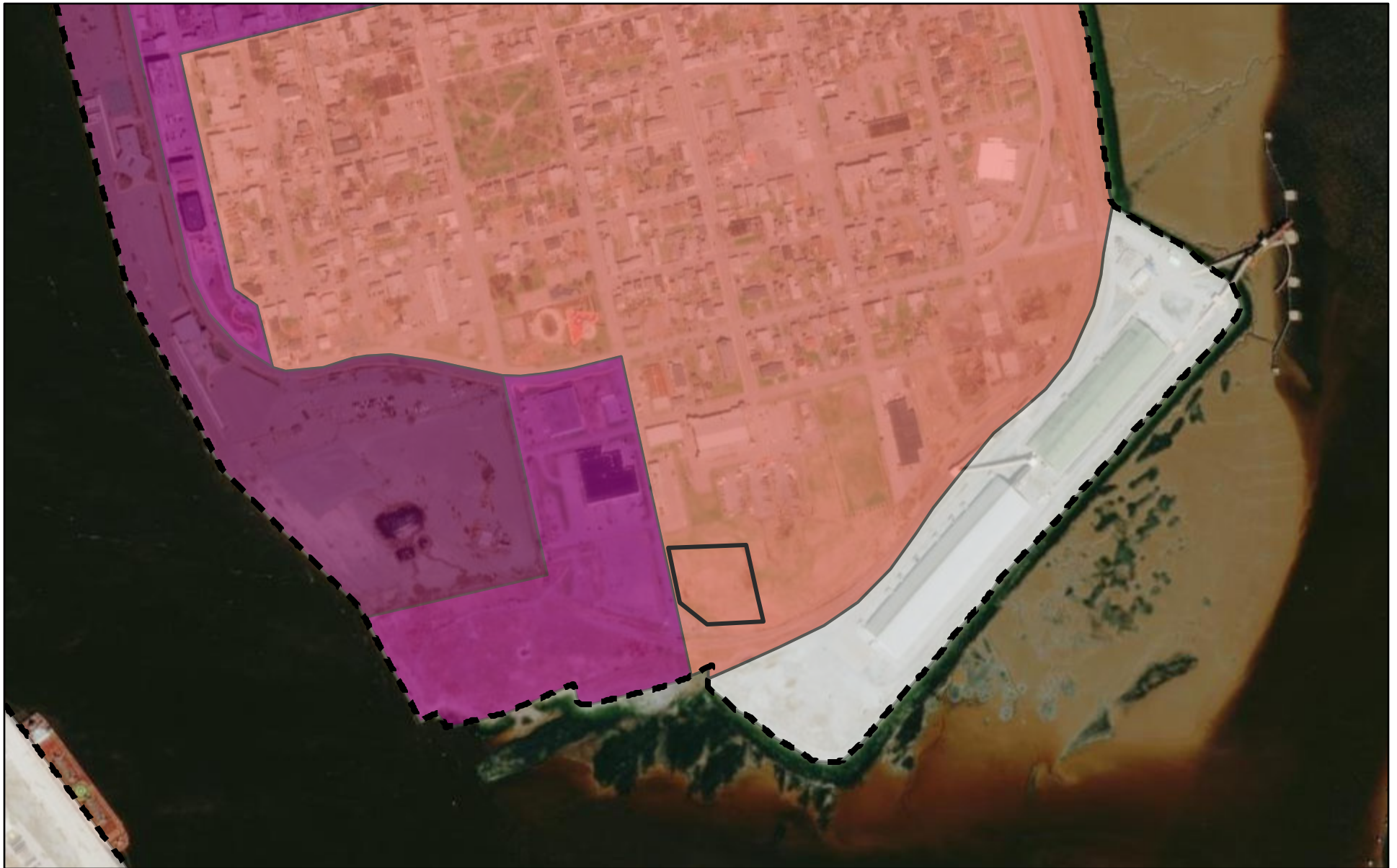
2021-04-08

- Civic Addresses
- ★ Conditional Zoning
- Subject Property
- Property Parcels



Esri Community Maps Contributors, City of Saint John, Province of New

Future Land Use 300 Sydney Street





2021-04-08

 Property Parcels

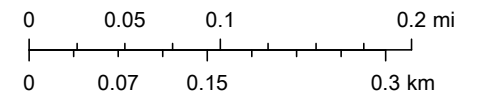
 Primary Development Area

Future Land Use

 Medium to High Density Residential

 Primary Centre

1:9,028



GeoEye, Maxar



View of site looking north.



View of site from Sydney Street.



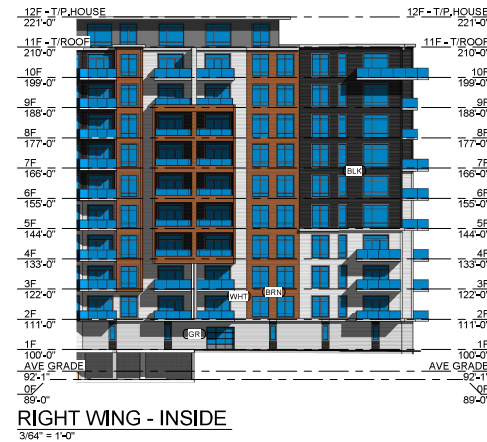
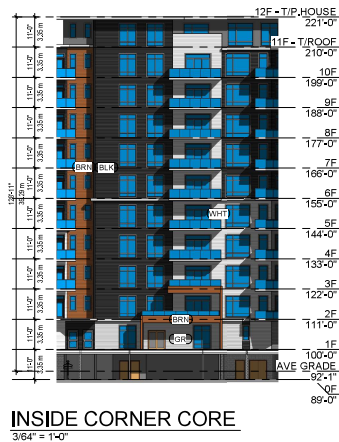
View along Sydney Street frontage of site.



View of Ordinance Building north of site.

Site Photography – 300 Sydney Street
Parsi Development NB Ltd.

City of Saint John
2021 April 8



"Not For Construction"

DATE	DESCRIPTION	DATE
01/11/2020	1.0	

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ALL CONTRACTORS MUST CONFORM TO ALL REGULATIONS, MUNICIPAL AND PROVINCIAL BY-LAWS AND THE NATIONAL BUILDING CODE OF CANADA.

ALL REQUIRED PERMITS MUST BE OBTAINED PRIOR TO ANY CONSTRUCTION.

Rev	Description	Date
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MATERIALS

EXACT MATERIAL COLORS SPECIFIED BY OWNER

- BLK - BLACK FIBER CEMENT SIDING
- WHI - WHITE FIBER CEMENT SIDING
- GRY - GRAY FIBER CEMENT SIDING
- BRN - BROWN FIBER CEMENT SIDING
- CMU - CONCRETE MASONRY UNITS

NOTE: PROJECTIONS TO BE CLASSED WITH COMPOSITE METAL PANELS OR FIBER CEMENT PANELS

MATERIAL LEGEND

1/4" = 1'-0"

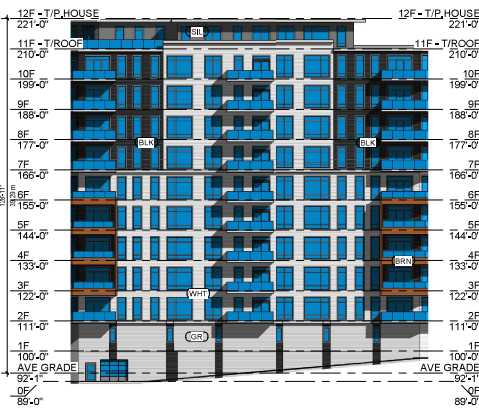
LEFT WING - END
3/64" = 1'-0"



RIGHT WING END
3/64" = 1'-0"



RIGHT WING - OUTSIDE
3/64" = 1'-0"



OUTSIDE CORNER
3/64" = 1'-0"



LEFT WING - OUTSIDE
3/64" = 1'-0"

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Architectural Designer

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Client

Parsi Development Ltd.

Project

Sydney St. Multi res.

300 Sydney St., Saint John

Drawing Title

ELEVATIONS

Date

Feb 1 2020

Drawn by

JP

Checked by

EM

Scale

AS NOTED

Sheet

ACIO

Sheet No.

4334















300 Sydney Street Third-Party Design Review

City of Saint John

Type of Document:

Final

Project Number:

SNB-21006624-A0

Prepared By:

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Discipline Lead – Architecture

Approved By:

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Date Submitted:

2021-04-09

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1 Overview

2021 is proving to be a momentous year for development in the City of Saint John – by all indicators, development activity and speculation has reached a fever pitch. Hundreds of new residential and commercial developments are under construction and more development plans are announced every day. 300 Sydney Street – a 12-storey, 180 residential building – is the latest and largest example of projects proclaiming their confidence in the Saint John market. It's an exciting time in Saint John's growth and transformation.

In March, EXP Architects Inc. was engaged by the City of Saint John to undertake a third-party design review of the proposed development at 300 Sydney Street. While the staff at the City of Saint John are undertaking the necessary municipal planning reviews, EXP's scope of work is focused on review of the urban design and architectural components of the project in relation to the City's Municipal Plan, Secondary Plan, and Zoning Bylaw.

1.1 Reviewed Documents

Documents from the developer, provided for EXP's review, included the following:

- Plans and elevations
- Various exterior renderings
- Shadow study video
- 2004 restrictive covenants attached to sale of land
- Report in support of zoning variances
- Site plan
- Contextual plan

In addition to the above, the latest versions of the following policy documents were viewed at the City of Saint John's website up to the date of this report:

- Municipal Plan: Plan SJ
- Secondary Plan: South Central Peninsula Neighbourhood Plan
- Zoning Bylaw: Zone SJ

1.2 Development Overview

The development is located at 300 Sydney Street and is among the southern-most development areas on the Southern Peninsula, immediately adjacent to Tin Can Beach. The lot is presently vacant and surrounded primarily by a mix of undeveloped lands and parking lots, as well as low-rise industrial / office buildings and heritage gems including the Brunswick and Armories. The potash terminal is next door, as well as small-scale 1- and 2- storey multi-unit residential buildings.

The proposed development is a V-shaped residential building hugging the westerly and southerly borders of the property. The building is proposed to include 12 storey's, 180 residential units, 2-levels of interior parking, and rooftop amenities. The design is decidedly modern and articulated with a variety of complimentary materials proposed in 4 different colours. A large parking lot covers the remaining portions of the property.

The project proponent is Parsi Developments and the project designer is listed on the supplied drawings as Spitfire Designs Inc.

2 Design Review

2.1 Initial Observations

The initial impression of the building is a significant project for the location and site. Not only is the height and massing a departure from the surrounding context, but the building's height – at nearly 40m – will be among the tallest buildings in Saint John and in the entire Province of New Brunswick. This is not to say that the project cannot be successfully designed and built for this site, however reviewing its design proves challenging as it challenges most of the intended design guidelines for the site and adjoining neighbourhood. This project could prove to be catalytic to the area and set a precedent that may require renewed planning and consideration of the surrounding area. A considered review and sensitive design will be necessary as a project of this nature will be precedent setting and leave a legacy for possibly 50 years or more.

As presented, the project has a very modern appeal. The material palette is intended to be, according to the designer, a combination of fibre-cement panels and siding, and composite metal panels. This generally aligns with the direction of the zoning bylaw. Without any physical breaks and setbacks in the massing as required by the policies, the cladding design and balcony arrangements have been thoughtfully arranged in an attempt to break down the scale and mass of the facades. The use of black cladding in the upper stories is particularly useful to bring focus to the lighter areas of the façade.

A high level of glazing and balconies at most units is intended to maximize sweeping views across the Bay of Fundy. The design proposes amenity spaces on the rooftop as well as what appears to be some rooftop plantings as shown in the renderings. Some additional thought should be placed around the quantity of high-elevation outdoor space given the windy, cool, ocean-front location; residents may not benefit as intended from these amenities in this particular location. Without proper landscape design and species selection, rooftop vegetation may have difficulties surviving the exposed conditions.

Additional amenity space is planned for the southern side yard setback which, according to the designer, is intended to be “a simple landscaped area to buffer out the railway, however this space can be used as a spot for tenant recreation, and a space for pet owners to let their companions out.” No fixed seating or “pocket park” design is intended, however, given the comments above regarding resident comfort at elevation, perhaps additional sheltered outdoor amenities can be provided in this location.

2.2 Opportunities for Improvement

The role of the third-party design reviewer can be difficult and fraught with the persistent desire to design the project themselves. EXP, however, recognizes this distinction and presents the following analysis as “opportunities for improvement” rather than specific design solutions. It is also recognized that there are many solutions to the same problem and the way the project designer may choose to address the opportunity may be entirely different from the design reviewer. As such, and though the initial reaction may be to take these opportunities for improvement as criticisms, the goal is to facilitate dialogue and thoughtful design consideration to achieve the best result for both the project proponent, residents, and the community that will host the project for decades to come.

2.2.1 Design Excellence

The term “design excellence” is used innumerable times throughout the reviewed policy documents. It sets an intention even if the term itself is relatively indefinable. What is excellent design can change from person-to-person and design-to-designer; it's entirely subjective. That said, many have tried to put some semblance of parameters around the term in order to attempt to measure the success of individual designs. One favourite definition:

“Excellent design must be contextual: buildings must work in harmony with their surroundings, and with nature. It should also be challenging – encouraging viewers and users to discover something new

about themselves, their surroundings and their interactions with the built environment – and challenging architects to explore new ideas, and new limits. Buildings should inspire a sense of awe and wonder. Design excellence must be seen as both the necessary investment in quality of life and a key factor in the health of our communities.” – Toon Dreessen, OAA, FRAIC, AIA, LEED AP

Clearly, there is much to consider and meeting that threshold requires a level of design uncommon to most similar development projects, historically. In addition, whereas this proposed development pushes or exceeds much of the boundaries set out in the policy documents, there is very little concrete criteria through which to review this project. Whereas the policy documents do not define design excellence, the spirit and holistic view of this example was used to guide the design review process in addition to the spirit and intention of the policy documents.

Specific to the proposed develop, perhaps the most overlooked component of the design is its surrounding context. In many ways, this development could be parachuted into any community and exist anywhere. The goal of the policy documents, however, is that all development should unequivocally and unmistakably be seen as a distinct “Saint John building.” Incorporating the local context into the design is where the difference – and design excellence – lies. With 2-stories of essentially blank facades at the street and a primary entrance located interior to the site, the design appears to have a distinct focus inward and almost intentionally avoid any connection to the surrounding context.

This project will set the tone for future development in the neighborhood and must recognize the catalytic effect it will have on future waterfront development in the area. The proposed development lacks a degree of self-awareness and deference; both to the neighbourhood and its long-term impacts.

Opportunities:

1. Engage Sydney Street and create a more welcoming public face integrated into the community.
2. Relocate the main entrance lobby to the Sydney Street-facing façade.
3. Improve the pedestrian experience along the Sydney Street corridor, perhaps by lining the space between the street and structured parking with live-work units, small commercial spaces, such as an ice cream shop or a small gallery to support Tin Can Beach and building residents, and glassy circulation or amenity spaces to show internal activity at the street levels.
4. Incorporate more active uses at the ground floors and add plenty of windows to allow more “eyes on the street” to promote safety of residents and the general public.
5. Explore design opportunities to incorporate the surrounding context, such as datums from the adjacent Brunswick, materials or colour palettes, planned Harbour Passage extension design language, native plant species, etc.

2.2.2 Context

2.2.2.1 Height + Scale

The height and scale of this proposed development exceeds the parameters envisioned in the policy documents. As such, it is important for the design to minimize, where possible, of reinforcing its own height and scale. Much of the design as proposed has used material treatments effectively in breaking down the bulk of the building, particularly the use of black on the upper levels of the building. This façade articulation strategy is more effective on the water-facing sides but could use some improvement on the other façades particularly in consideration of future integration of the site into planned public recreation spaces on the adjacent Armory site.

That said, a defining horizontal element – a strong base – projected away from the main tower at a more appropriate human scale of 2- to 3- storeys may help to ground the building and avoid overt expressions of verticality. With such a base at the street, it will be more difficult to sense the overall height with an appropriately scaled and contextually integrated street wall.

It is clear that the project proponent has taken into consideration the effect the building's height and mass will have on sun and shadows to adjacent properties; the provided video, assumed to be properly calibrated to the selected site, appears to indicate that the bulk of the shadows will fall within its own property as a result of the building's alignment with the sun. It was not clear, however, the time of year that the shadow study video was based upon. December 21st, the winter equinox, is the shortest day of the year when the sun is the lowest in the sky and, consequently, creates the longest shadows when sun is minimal but in high demand. If not already presented, this scenario should be well understood before advancing with design and construction.

Wind, particularly with tall buildings, is another environmental condition that requires some degree of study. The proponent has indicated that no level of wind study has been completed to date. Though there are not any anticipated wind tunnel effects given the low height of surrounding buildings, future development at heights anticipated in the neighbourhood plan may create future wind tunnels. In addition, other wind effects such as vortex shedding, bluffing, and negative air pressure may have unintended consequences for neighbouring buildings and this development. The potential for building vibration, whistling, low-scale wind vortexes, "flying" furniture, and snow drifting should be well understood, particularly since the broad side of the building directly faces the prevailing wind direction throughout the year and will take nor'easters head on.

Lastly, regarding the building's extra height beyond the policy documents, there is a very clear stipulation in BD-6 of the neighbourhood plan that indicates the additional height should only be provided when there is a clear public benefit. From the project as-designed, there does not appear to be any direct public benefit derived through the design itself, however, it is our understanding that the proponent has entered into discussions with the City of Saint John regarding additional on-site and off-site public benefits that can be used to offset this requested additional height. We are supportive of these discussions and recommend the same offsetting approach. Providing amenities or improvements to nearby public amenities, such as Tin Can Beach, Harbour Passage extension, or Rainbow Park may be effective offsets.

Opportunities:

1. Improve façade articulation strategy on the rear facades to better match the water-facing sides.
2. Create a well-defined horizontal base at an appropriate human scale that engages the community and improves the pedestrian experience of the building's height.
3. Understand shadow impacts on adjacent properties using the winter solstice as the baseline.
4. Complete a wind study to understand the effects of the proposed design on the development and adjacent properties.
5. Pursue additional on-site and off-site public benefits to offset additional requested height variance.

2.2.2.2 Waterfront Prominence

A key site, the policy documents clearly position Sydney Street as a character corridor, providing entry to the South Waterfront, and is a major point of terminus at the ocean. If that were not enough, the site and proposed development are prominent enough to be considered the gateway to Saint John for visitors arriving by sea – more

than 200,000 people in a normal year experience this vista. An extension of the popular Harbour Passage is planned for this stretch of Sydney Street. The pressure for design excellence in this respect is palpable.

The space in front of the development is a key arrival space, and the only public point of access to the water at present. Rather than engage with this public space, the proposed development presently appears to ignore its highly visible location completely by turning its back to the street and becoming an insular development that doesn't actively engage with its context and neighbourhood as a building of its prominence should.

At present, the proposed design negatively impacts the streetscape and quality of existing public space. As described in the sections above and below, the development, particularly at the street level should be turned outward and consider its role as both a prominent terminus and gateway on an important street.

Opportunities:

1. Explore opportunities to enhance the design of the building considering the site's planned future prominence rather than its present lack of prominence.
2. Understand plans for the proposed Harbour Passage extension and integrate design features and clear connections to it into present plans for the building.
3. As described in sections above and below, improve the pedestrian experience at ground level that supports and integrates with Tin Can Beach as an important community-scale amenity and vista.

2.2.2.3 Heritage

Though not located in a heritage district, some immediate neighbours to the property, specifically the Brunswicker and Armory, have special recognized historic significance. Saint John is characterized by its high degree of intact heritage structures and, as the City continues to develop on its 235+ years of growth, it is important that our new developments do not overshadow and diminish that history.

That said, we also must be cognizant of our role in creating new history too. Consequently, the expectation isn't that this or any other new develop mimic or slavishly bow to this heritage but that new buildings should understand their context, stand proudly on their own design laurels and give a nod to the history. At present, and as described above, the proposed design all but ignores the surrounding context.

A proposed projected base, as described above, could align with datums derived from the Brunswicker providing needed neighbourhood context. Material palettes could be inspired by those of the Brunswicker and Armory, either in colour, material, or finish. Punched openings of the same scale or fenestration pattern could be carried along in a contemporary way as well to give a modern nod to these heritage neighbours.

Opportunities:

1. Design a human-scaled base to the building that takes its design cues from the adjacent Brunswicker and (less so) the Armory, such as datums, material palette, colour palette, design language, or design features.

2.2.2.4 Setbacks and Step-backs

The provided site plan indicates that the building is intended to be set back 6m from Sydney Street in accordance with the proposed zone standards (RH) but policies elsewhere within the policy documents suggest that setbacks are to match the existing context where the context does not comply with the present standards, such is the case here.

The adjacent Brunswicker on the same side of the street and block has zero front setback and immediately abuts the sidewalk in the street right-of-way. When only 1 building exists on the block face, the policies recommend averaging between the required setback and the setback of the adjacent building. This would typically be used to justify future buildings being set back at the required setback. Seeing as this development is the last building on the block and the street terminates at the ocean, there is no further context to be supported by averaging the setbacks in this way. It is our recommendation that the building match the zero setback of the Brunswicker to make a consistent street face on this final block of Sydney Street.

Using the above setback rationale as the line of measurement of the intended step-backs of the façade per the zoning bylaw, the building, as presently located, is effectively set back from the street the required 6m albeit using a different methodology than the one proposed in the zoning bylaw. Considering this observation i, it is suggested that a horizontal building base of 2 to 3 stories extend to the property line while the remainder of the tower above the second or third storey remain at 6m from the property line as shown in the site plan. These approaches combined may give substantial deference to the surrounding context and simultaneously reduce the perceived building height.

Opportunities:

1. Adjust the front yard setback to within 1m of the sidewalk to align the proposed horizontal base with the adjacent context provided by the Brunswicker.
2. Retain the remainder of the tower above the proposed base at the 6m setback with no further step-backs.

2.2.2.5 Pedestrian Experience and Community Connection

At the risk of repeating some of the discussion from the sections above, the design of the building should be used to reinforce the human scale and sense of place, particularly as a terminus of Sydney Street and the intended prominence of Tin Can Beach as an important neighbourhood amenity.

While the restrictive covenant in place on the site that restricts the use of the ground floor to non-residential uses is recognized, the design response to the first and second floors as structured parking – a largely blank, black wall – is wholly inappropriate to the goal of animating the public realm and providing a human scale. Not only does the present design ignore the activation and connection needed at the street level, with nothing to cast the eyes on at the street level, the eye will gravitate upward and focus on the overwhelming height of the building. Instead, the base should provide a sense of place and a scale that is easy to relate oneself to.

In addition to the strategies explored above, including relocating the primary entrance to Sydney Street, other strategies can be explored to provide a human scale, such as reinforcing entries and exits with canopies or landscaping to provide a sense of arrival, providing active programming and glassy areas to wrap the structured parking, or providing small-scale rentable commercial opportunities like an ice cream server, mini art gallery, coffee shop, or meeting room, as examples.

Opportunities:

1. In the design of the proposed horizontal base described elsewhere, explore opportunities to provide human scaled elements, such as landscaping or canopies at key entry points, such as the proposed relocated main lobby entrance, and appropriately designed drop-off zones.
2. Relocate some building amenity spaces, glassy circulation spaces (such as stairwells and corridors), live-work units, and / or small-scale commercial uses to the lower floors to animate the streetscape.

2.2.3 Transportation + Parking

There is broad agreement among the policy documents to reduce the level of auto dependence in Saint John and promote alternative forms of transportation, such as cycling, walking, or transit. The southern peninsula presently has the highest share of users of alternate means of transportation thanks to a tight grid pattern of connected streets, sidewalks, and bus service and all the needs of a complete community at hand. As such, it can be argued that there is less demand or need for parking in this area of Sydney Street. The policy documents account for this by offering broad parking exemption areas throughout parts of the peninsula and reductions of up to 20% on parking requirements in intensification areas. Furthermore, all parking lots of 100 cars or more are limited to a 125% maximum above the base requirement to reinforce this directive.

The building site is located within the southern peninsula intensification area and the building is required to have more than 100 parking spaces per the RH zone standards. While the project proponent has proposed a parking ratio of 119% that is within the stated limits of 80% and 125%, we encourage the proponent to take additional efforts to reduce this figure in the spirit of the policy documents. The rear of the property and remainder of the site is dominated by surface parking in an area that can easily support reduced parking requirements. In addition, the policy documents indicate that the eastern side of Sydney Street is included in the parking exemption area for buildings that, at present, are intended to have a lower density and be half the height of the proposed building at 300 Sydney Street. As such, there is a great possibility that future density in the area will provide additional amenities that could further reduce the need for auto dependence. Lastly, on vehicle parking, residents would be eligible to purchase low-cost on-street parking passes as an alternative to onsite parking or off-street parking across the street at the Vulcan Street parking lot.

In this vein, further parking reductions within the existing bounds of the policy documents can be valuable to reduce structured or surface parking to either reduce the height of the building and associated expensive structured parking costs or reduce surface parking area costs. These cost reductions could be used to add design features or amenities to the project, such as bio-swales and stormwater retention ponds with native vegetation, reduce rent costs to increase competitiveness, or speed project financing due to reduced construction costs.

Beyond vehicles, as described in the sections above, there is a great potential to connect the building to its context and further encourage alternative means of transportation. Having the primary building entrance located on Sydney Street as well as having outdoor bicycle parking amenities for residents, visitors, and the public would encourage active transportation. A bus stop is located at the corner of Sydney Street and Broad Street, however, at present the site design and building access points reinforce and encourage automobile usage. Furthermore, the planned extension of Harbour Passage, a highly desirable active transportation corridor of increased design value, is intended to pass by the front of the development and direct connections to it will encourage building residents to use it regularly and access the alternate means of transportation throughout the southern peninsula.

Opportunities:

1. Reduce parking counts toward the lower end of allowable limits to reduce building scale or add design features or site amenities.
2. Relocate high visibility, articulated pedestrian entries onto Sydney Street.
3. Relocate some of the proposed interior bicycle parking infrastructure outside accessible to users of Sydney Street.
4. Relocate interior bicycle parking to be within 15m of the primary entrance on Sydney Street.
5. Integrate the future extension of Harbour Passage into the design of the building.

2.2.4 Landscaping + Sustainability

20th century architect Frank Lloyd Wright famously quipped “a doctor can bury his mistakes, but an architect can only recommend his clients plant vines.” This quote isn’t intended to say that this proposed development is a mistake in any way, but the underlying truth of this quote is that landscaping is a valuable method of softening architecture into its surroundings.

In the case of a building, such as that proposed here, that far exceeds the existing height and density context planned for the area, heavy investment in landscaping can soften its impacts, help it blend into its surroundings, and create an engaging, wonderful pedestrian experience. The same is true for required planting areas in the surface parking lot to break up the expanses of asphalt. East Point Shopping is an intriguing example where landscaping was used to great effect. Harbour Passage, planned to pass the doorstep of the development, has seen its success due to robust landscape design which this project should plan to emulate. Sydney Street, deemed a character corridor in the policy documents, further reinforces the need to increase the level of design and landscaping at the street level.

Stormwater management designs have not been prepared to date so it is not possible to comment on the planned strategy. Reductions in surface parking, as described above, lends an opportunity to use interesting landscape design, such as bio swales, retention ponds, and native plant species, that will reduce the visual impact of the surface parking. This modification could provide interesting and dynamic landscaping opportunities that are more in keeping with the direction of the neighbourhood plan and are far more interesting to use and look over for residents outside and in the upper stories of the rear façade.

The documentation provided by the proponent is not sufficiently detailed at this stage to identify sustainability strategies or material selections, however, as described in the use of the term “design excellence,” sustainability is a major element in the policy documents and an important consideration in all design work in light of the unprecedented climate emergencies that are happening across the globe. Though selecting a rating program, such as LEED, Green Globes, PassivHaus, or BOMA Best, as examples is strongly encouraged, designing in their spirit remains a distinct option. In accordance with the City’s desire to see buildings sustainably built developments, the proponent should place a high degree of effort in reducing the building’s environmental footprint and demonstrate how the development exceeds the minimum requirements of current building codes.

Opportunities:

1. Increase the areas of landscaping and level of landscape design, similar to that of Harbour Passage and East Point Shopping.

2. Explore opportunities to reduce surface parking areas to introduce additional landscaped area that can double as stormwater retention areas.
3. Demonstrate a high degree of environmental sustainability in the building design over and above minimum design requirements.

3 Trade-off Opportunities

Undertaking a development of this size and scale takes guts. And a lot of money. Consequently, despite the opportunities described in Section 2 above, the intention is not necessarily to make the development more expensive. Refocusing investment in one area of the development to another is a prudent method to conserve costs. As such, the table below represents opportunities to shift existing costs to areas that will have outsized impact for residents, the public, and the developer. The goal is to achieve design excellence for all involved.

1. Reduce surface parking area and total parking spaces to minimum levels and divert those costs and freed-up land into enhanced stormwater management landscaping, such as bio-swales, a retention pond, and park space for residents.
2. Eliminate the drop-off zone at the present main entry and utilize Sydney Street and the existing cul-de-sac as an on-street lay-by area; savings could be allocated to improving the pedestrian experience with entry canopies, windows, plantings, etc.
3. Reallocate existing program elements, such as some rooftop amenity spaces and residential units, to the proposed horizontal base thereby reducing the height of the building and increasing security with more “eyes on the street.”
4. Reduce the interior bicycle storage area and replace the lost space with bicycle racks and exterior storage compartments thereby increasing floor space for additional valuable parking or storage.
5. Relocate the main entrance to the proposed horizontal base to free up area within the tower for an additional residential unit
6. Eliminate interior elevator lobbies on each floor in favour of a smaller in-corridor space to free up floor space for additional residential units that could be eliminated from higher levels and reduce the proposed building height
7. Relocate exit stair shafts into the proposed horizontal base to free up floor area for larger residential units.

4 Summary

The proposed development at 300 Sydney Street, at first blush, is an ambitious project that exceeds all anticipated uses at that site. Upon further review of the designs, with some adaptations to address its local context and improving the human scale, as described in the sections above, it is our opinion that the project can be successfully incorporated into the neighbourhood and sets a good precedent to attract even density toward the southern waterfront area. Incorporating the proposed opportunities for improvement above as well as the current negotiations with the City of Saint John regarding offsite and onsite improvements show a clear benefit to the public. In addition, using some or all of the proposed trade-off opportunities should minimize or mitigate some of the costs to implement the opportunities for improvement.

By incorporating many of the opportunities for improvement described herein may fundamentally change the building aesthetics as presently proposed. Plans, elevations, and / or renderings should be resubmitted to the Development Officer for their review and acceptance well before the issuance of any building permits to ensure these improvements are satisfactory and in keeping with the spirit of this report and the policy documents.

We remain confident in the future of the City of Saint John, particularly with the confidence of developers such as Parsi willing to invest in our city. We look forward to seeing this development proceed forward in the best interest of our entire community.

We are committed to responding to any questions, clarifications, and explanations needed to clarify anything contained in this report by anyone, including other City departments, the developer, and the public. Please do not hesitate to reach out to us at any time.

5 Notes

- This report in no way represents a technical review of the design or feasibility of the project and is limited to general discussion of the building's aesthetic appeal and adherence to the policy documents relied upon herein. The technical design remains the sole responsibility of project architects and engineers engaged by the proponent to design the building and must be licensed to practice in the Province of New Brunswick.
- EXP accepts no liability for the City of Saint John or the proponent's reliance on the recommendations outlined herein.
- Nothing contained herein shall be considered binding upon the proponent unless so determined by the Development Officer, Planning Advisory Committee, or Common Council, as the case may be, within the extent of their authority provided by the Community Planning Act and elsewhere.
- Many of the policy objectives contained in the policy documents are subjective in nature and cannot be easily qualified or quantified. Disagreement may arise in the interpretation of these elements and the decision as to the appropriate interpretation should be made by the Development Officer, Planning Advisory Committee, or Common Council, as the case may be, whose decisions shall be considered final and binding in accordance with the powers granted by the Community Planning Act.

300 SYDNEY STREET TRAFFIC IMPACT STATEMENT

Traffic Impact Study
Proj. No.2101000

February 17, 2021

Revision No.: 0

Spitfire Design

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Appendix B:Traffic Count Data

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1 INTRODUCTION

1.1 PROJECT BACKGROUND

A new 12-storey residential development has been proposed at the southern end of Sydney Street in the City of Saint John. The development will consist of 180 units (45 three-bedroom, 45 two-bedroom, and 90 one-bedroom) as well as a two-level underground parking garage, a surface parking lot, and rooftop amenities. The proposed development site plan, which is included in **Appendix A**, shows 186 parking spaces, including 75 surface level spaces and 111 underground spaces. The plan also includes 6 barrier free spaces – 2 underground and 4 at surface level. The surface level parking lot and second level of underground parking will be accessible through the main development access off Sydney Street, just north of the building. A secondary access will be located at the centre of the building to the south of the main access and will provide access to the first level of underground parking. The proposed development location is shown below in **Figure 1**. Site photos of existing conditions are shown in **Figure 2**.

As part of the development approval process, the City of Saint John has requested that a Traffic Impact Statement (TIS) be completed for this development. The primary concerns are how the development will impact traffic at the intersection of Sydney Street and Broad Street and whether upgrades should be considered to improve traffic flow and pedestrian safety. Spitfire Design retained Englobe Corp. to complete this TIS. The Study Area for this TIS includes the intersection of Sydney Street and Broad Street as well as the section of Sydney Street between Broad Street and the proposed development site.

Figure 1 – Site Location Plan

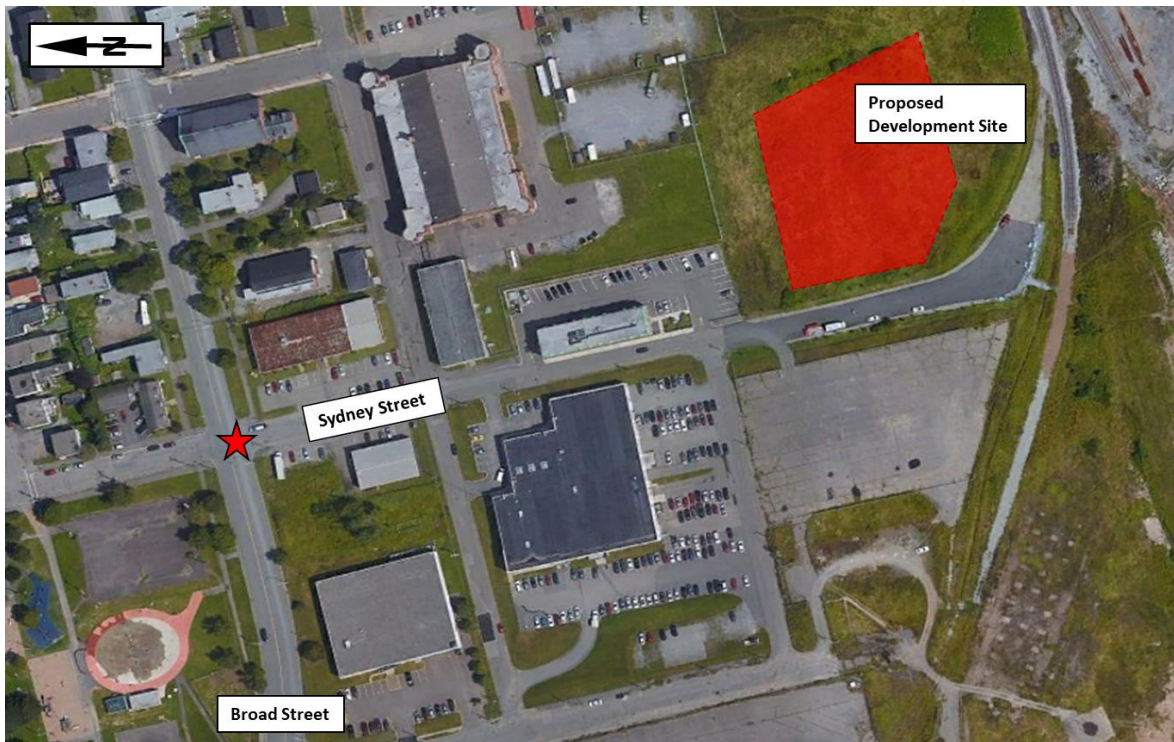


Figure 2 – Site Photos



Proposed Development Site, looking north on Sydney Street



Sydney Street at Broad Road, looking northeast



Proposed Development Site, looking southeast

1.2 STUDY TASKS

The main objectives of this TIS were to estimate how much additional traffic the residential development would generate and determine what impact, if any, the development traffic would have on the intersection of Sydney Street and Broad Street. The following activities were undertaken as part of this TIS:

- Englobe staff visited the study area to review existing conditions;
- Existing 2021 traffic volumes were collected at the Sydney Street / Broad Street intersection. These volumes were then adjusted to take into account the impact of COVID-19 on existing traffic patterns;
- A 1.0% exponential annual growth rate was applied to estimate the future 2028 background traffic volumes for the Study Area. This year represents the 5-year horizon period beyond the anticipated full build-out of the development;
- ITE Trip Generation rates were used to estimate the amount of traffic that will be generated by the new development. These were added to the background traffic volumes to estimate the 2028 total traffic volumes with the development in place;
- LOS analyses were completed for the 2028 future conditions with full build out of the development;
- A traffic signal warrant analysis was completed at the intersection of Sydney Street and Broad Street;
- A review of existing pedestrian infrastructure was performed to identify the need for improvements;
- The methodology, findings, and recommendations of the TIS were documented in this written report.

1.3 HORIZON YEAR

A 5-year horizon period was utilized for the analysis. Should all approvals be granted it is expected that the proposed development will be fully operational in 2023, therefore 2028 was chosen as the future horizon year for the analysis.

2 INFORMATION GATHERING

2.1 STREET AND DEVELOPMENT CHARACTERISTICS

Sydney Street is a local street that is oriented in the north-south direction. Within the Study Area, it features one lane in each direction and has a speed limit of 50 km/h. North of Broad Street, Sydney Street features curb and gutter as well as asphalt sidewalk on both sides of the street. South of Broad Street, Sydney Street is not curbed and features sidewalk that extends along the west side of the street between Broad Street and Broadview Avenue and along the east side of the street between Broadview Avenue and the proposed development site. There is no sidewalk directly in front of the development site.

Broad Street is a collector street that is oriented in the east-west direction. It features one lane in each direction and has a speed limit of 50 km/h. Near Sydney Street, Broad Street features curb and gutter as well as sidewalk on both sides of the street.

The intersection of Sydney Street and Broad Street is a standard 4-leg, 2-way stop-controlled intersection. Broad Street is free flowing and stop signs are present at the Sydney Street approaches. Although both streets feature sidewalks, no marked crosswalks are present at the intersection.

2.2 TRAFFIC DATA AND COVID ADJUSTMENTS

New (2021) traffic data were collected by Englobe at the intersection of Sydney Street and Broad Street. Traffic counts were completed during the mid-day and evening peaks on Tuesday, February 9th 2021, and during the morning peak on Wednesday, February 10th 2021. The traffic count data are provided in **Appendix B**.

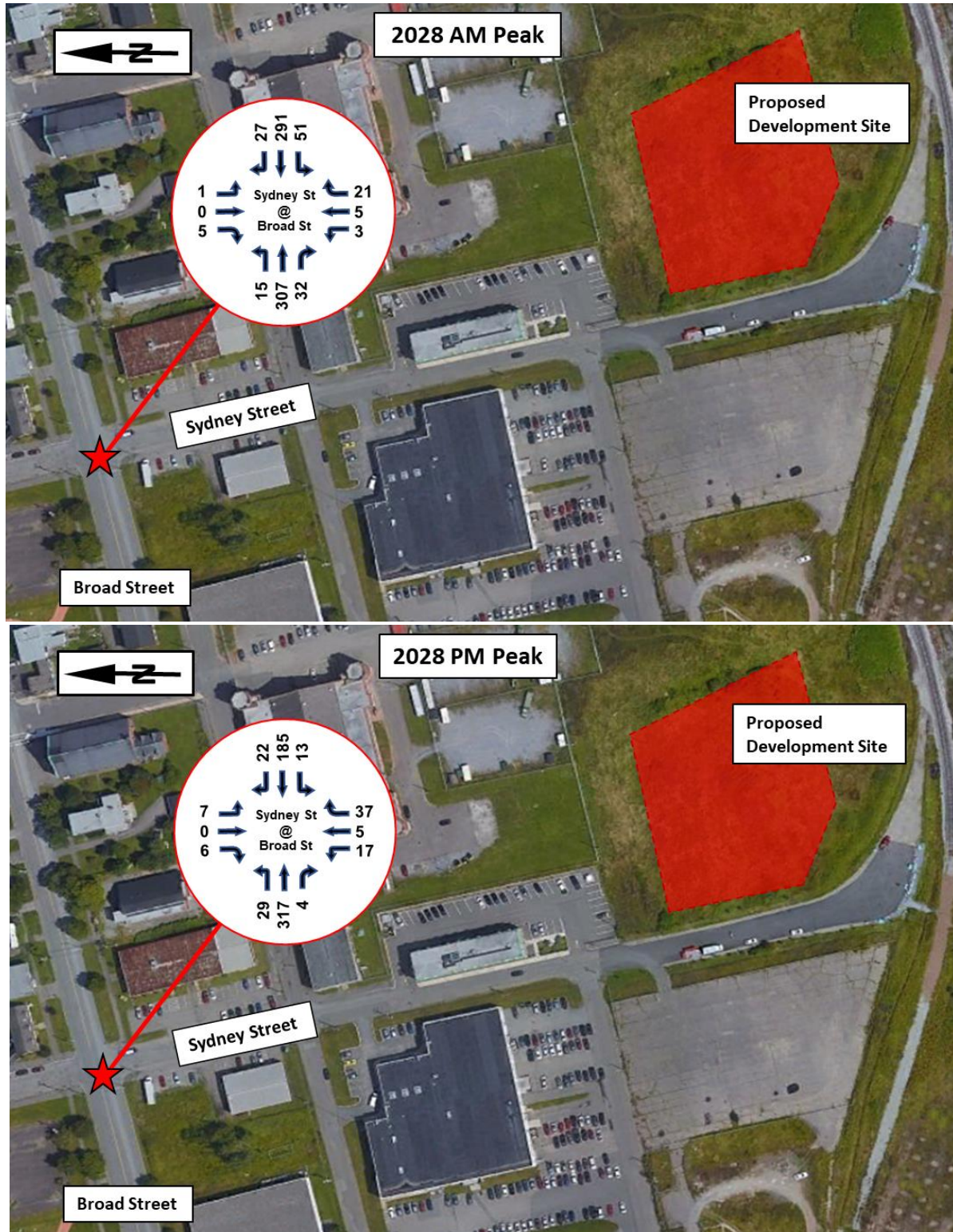
Since traffic patterns have decreased as a result of the current COVID-19 pandemic, the Study Team determined that any traffic count data collected as part of this study should be adjusted to better represent typical traffic volumes under normal conditions. Intersection traffic volumes were adjusted using factors that were developed and used in a traffic study performed by the Study Team in January 2021. This study compared traffic data that were collected in 2016 at two locations in Fredericton, NB to actual traffic volumes from the same locations that were counted during the COVID-19 pandemic. The average AM and PM peak hour adjustment factors were calculated for the two Fredericton locations and applied to the traffic data in this study. The Fredericton study did not include Mid-day peak adjustment factors, therefore the Mid-day adjustment factor was estimated by comparing the Mid-day peak hour traffic volumes to the AM and PM peak hour volumes and adjusting accordingly. The Mid-day peak adjustment factor was calculated by taking the average of the AM and PM adjustment factors and reducing it by 20%. The adjustment factors that were used in this study are shown in **Table 1**.

Table 1 – COVID-19 Adjustment Factors

Study	Date	AM Peak	Mid-day Peak	PM Peak
Fredericton, NB	January, 2021	1.26	-	1.20
Fredericton, NB	January, 2021	1.36	-	1.25
Applied to Saint John Study	February, 2021	1.31	1.21	1.22

The adjustment factors were applied to the peak hour volumes at the intersection. The adjusted 2021 volumes were then projected forward to the 2028 horizon year by applying a 1 % annual growth rate. The 2028 AM and PM background traffic volume estimates are shown in **Figure 3**. The Study Team also compared the adjusted volumes to a traffic count that was completed at the nearby intersection of Broad Street and Wentworth Street in 2018 and found that the volumes on Broad Street were consistent in both counts (the Wentworth intersection is located two blocks east of Sydney Street).

Figure 3 – 2028 Peak Hour Volumes without Development



3 TRAFFIC GENERATION AND ASSIGNMENT

Trip generation rates for the proposed development were estimated using the ITE TripGen Web-based App, which is based on the 10th Edition of the Institute of Transportation Engineer's (ITE) *Trip Generation Manual*. The Developer provided information regarding the size and type of development that is planned. The building will consist of 12 stories with a total of 180 dwelling units.

ITE Land Use #222 (Multifamily Housing – High-Rise) was used to generate trips for the development. The resulting vehicle trip generation is shown in **Table 2**. It was assumed that all of these trips would be made by motor vehicle as that would represent a worst-case scenario in terms of traffic generation. This is a conservative approach, as in reality there would likely be some trips made by other modes of transportation such as walking, cycling, or transit.

Table 2 - Traffic Generation for the Proposed Development

Development	Size	AM Peak Hour			PM Peak Hour			Daily Total
		In	Out	Total	In	Out	Total	
Multifamily Housing - High-Rise (ITE Land Use #222)	180 Dwelling Units	15	48	63	43	27	70	921

The development traffic was added to Sydney Street. It was assumed that all traffic would travel through the Sydney Street/Broad Street intersection to access the development. The generated traffic was distributed as shown in **Figure 4**. These distributions were determined based on the existing traffic volume distributions at the intersection.

The peak hour traffic volumes for the 2028 horizon year were estimated by adding the development traffic to the 2028 background traffic volumes. The 2028 traffic volumes with the development in place are shown in **Figure 5**.

Figure 4 – Development Traffic Distribution and Assignment

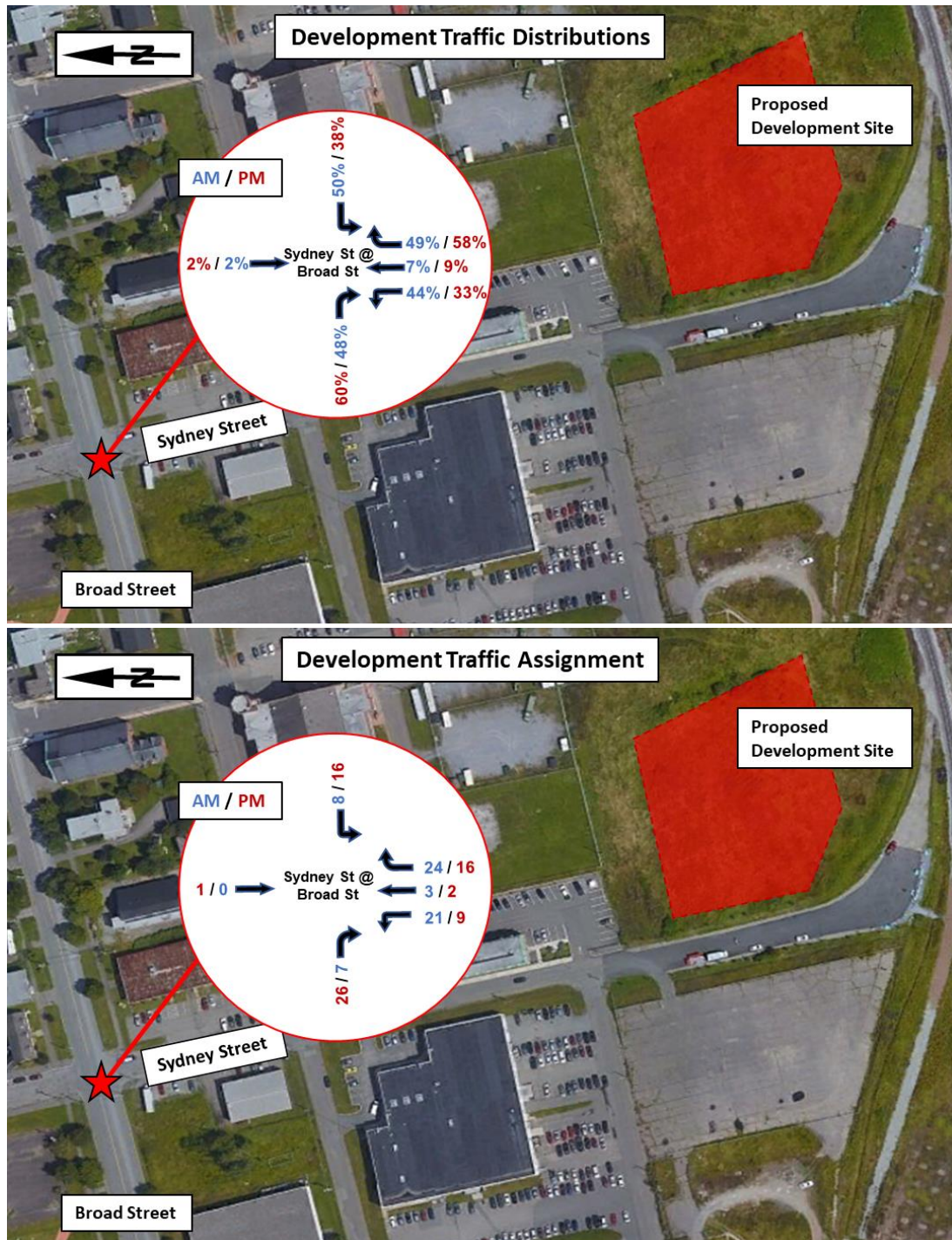
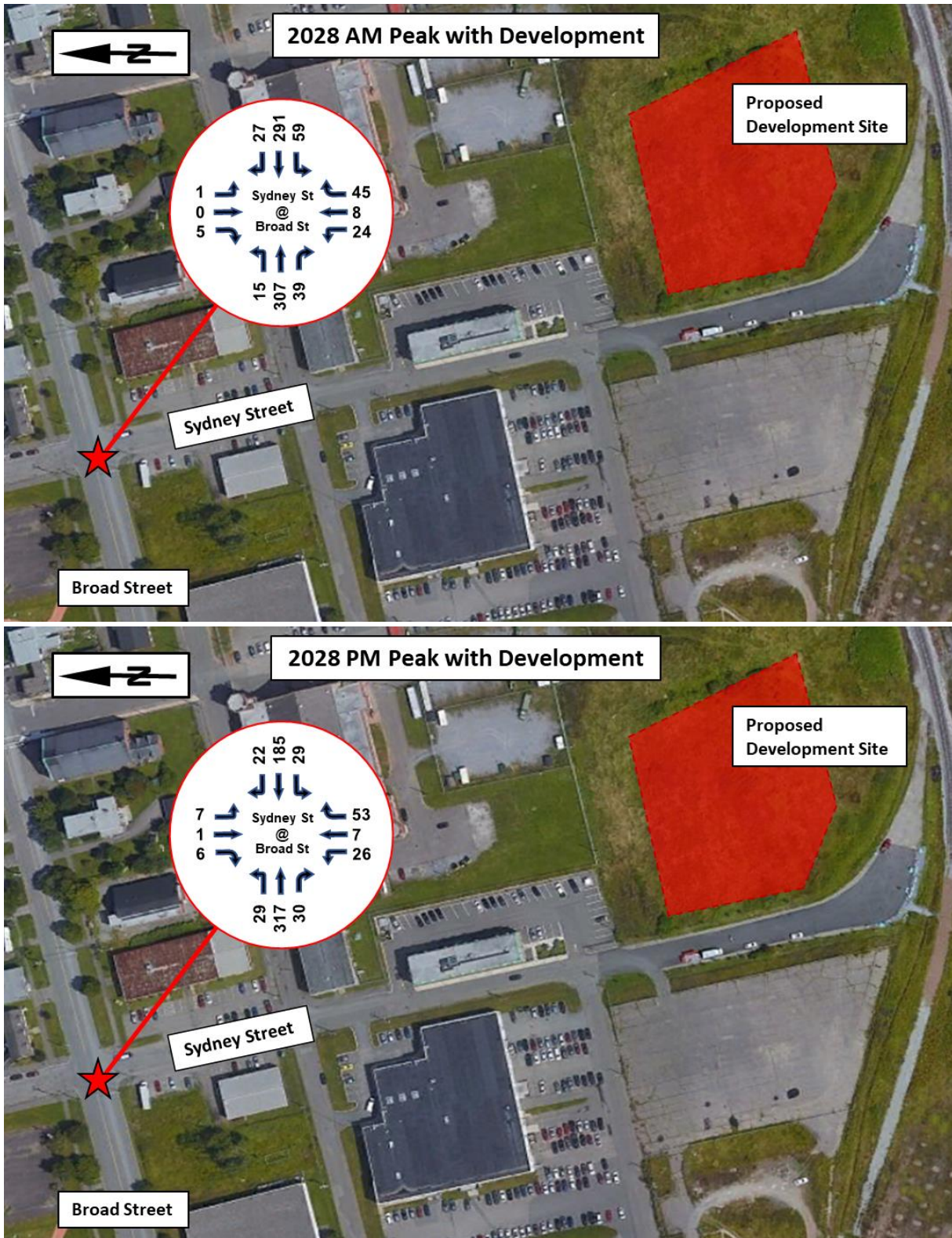


Figure 5 – 2028 Traffic Volumes with Development in Place



4 LOS ANALYSIS WITH DEVELOPMENT

A Level of Service (LOS) analysis was completed for the future 5-year (2028) traffic conditions at the intersection of Sydney Street and Broad Street with the development in place. The findings are discussed in this section.

4.1 LEVEL OF SERVICE CRITERIA

The LOS analyses were completed with Synchro 10, which is a traffic analysis software that uses the Highway Capacity Manual and Intersection Capacity Utilization procedures.

The intersection performance was evaluated mainly in terms of the level of service (LOS), which is a common performance measure of an intersection. The LOS is determined based on vehicle delay and is expressed on a scale of A through F, where LOS A represents very short delay (<10 seconds per vehicle) and LOS F represents very long delay (>50 seconds per vehicle at a stop controlled intersection and >80 seconds per vehicle at a signalized intersection). A LOS D is often considered acceptable in urban locations; however, some communities will accept a LOS E. The LOS criteria for both signalized and stop control intersections are shown in **Table 3**.

Table 3 – Intersection Level of Service Criteria

LOS	LOS Description	Control Delay (seconds per vehicle)	
		Signalized Intersections	Stop Controlled Intersections
A	Very low delay; most vehicles do not stop (Excellent)	less than 10.0	less than 10.0
B	Higher delay; more vehicles stop (Very Good)	between 10.0 and 20.0	between 10.0 and 15.0
C	Higher level of congestion; number of vehicles stopping is significant, although many still pass through intersection without stopping (Good)	between 20.0 and 35.0	between 15.0 and 25.0
D	Congestion becomes noticeable; vehicles must sometimes wait through more than one red light; many vehicles stop (Satisfactory)	between 35.0 and 55.0	between 25.0 and 35.0
E	Vehicles must often wait through more than one red light; considered by many agencies to be the limit of acceptable delay	between 55.0 and 80.0	between 35.0 and 50.0
F	This level is considered to be unacceptable to most drivers; occurs when arrival flow rates exceed the capacity of the intersection (Unacceptable)	greater than 80.0	greater than 50.0

4.2 LOS ANALYSIS WITH DEVELOPMENT


A Level of Service (LOS) analysis was completed for the 2028 traffic conditions at the intersection of Sydney Street and Broad Street. The existing traffic control was assumed for the analysis (stop signs on Sydney Street and free-flow on Broad Street). The LOS results are summarized as follows:

- In 2028, the intersection would operate efficiently at an overall LOS A during both peak periods.
- During both peak periods, all individual approaches would operate at a LOS C or better.
- The northbound approach would operate at a LOS C with average delays of 16.7 and 15.3 seconds during the AM and PM peak periods, respectively.

The LOS results indicate that the additional traffic generated by the new development will not create deficiencies at the intersection of Sydney Street and Broad Street. The overall delay and LOS are expected to remain acceptable up to 5 years beyond the anticipated full build-out.

The LOS results, including average delay, volume to capacity (v/c) ratios, and the 95th percentile queue lengths for the existing conditions are summarized in **Table 4**. Detailed Synchro analysis outputs are included in **Appendix C**.

Table 4 – 2028 LOS with Development

Intersection			Overall LOS, Delay (sec/veh)	Turning Movement LOS Average Delay (seconds per vehicle) [Volume to Capacity Ratio (v/c)] 95 th Percentile Queue (m)											
				Eastbound			Westbound			Northbound			Southbound		
East-West Street @ North-South Street	Traffic Control	Time Period		L ←	T ↑	R →	L ←	T ↑	R →	L ←	T ↑	R →	L ←	T ↑	R →
Sydney Street @ Broad Street		AM Peak	LOS A 2.7	Shared	A 0.5 [0.01] <1	Shared	Shared	A 1.8 [0.06] 1	Shared	Shared	C 16.7 [0.22] 7	Shared	Shared	B 12.3 [0.01] <1	Shared
		PM Peak	LOS A 3.0	Shared	A 0.9 [0.03] <1	Shared	Shared	A 1.3 [0.03] 1	Shared	Shared	C 15.3 [0.23] 7	Shared	Shared	C 15.6 [0.05] 1	Shared

5 TRAFFIC SIGNAL WARRANTS

The Study Team completed a traffic signal warrant using the TAC methodology, which is documented in the *Traffic Signal and Pedestrian Signal Head Warrant Handbook (2014)*. The methodology considers the following intersection characteristics:

- Six-hour turning movement and pedestrian volumes covering the AM, Noon, and PM peaks;
- Intersection geometry (lane configurations, spacing, right-turn slip lanes, etc.);
- Adjacent land uses (schools, mobility challenged citizens, senior citizen complexes, etc.);
- Distance of nearest upstream traffic signals;
- Population of community;
- Location within the community (central business district, etc.); and
- Percentage of heavy vehicles.

The TAC methodology determines the need for a traffic signal based on a priority point system using the characteristics described above. Each characteristic contributes toward the justification of a traffic signal. If the signal warrant generates 100 points or more then traffic signals are typically warranted.

The traffic signal warrant was performed for the intersection of Sydney Street and Broad Street for the 2028 horizon year with the development in place. A warrant score of 24 points was achieved for the intersection. The signal warrant results show that a traffic signal is not warranted at the intersection with the additional traffic that is predicted to be generated by the new development. The signal warrant worksheet is provided in **Appendix D**.

6 PEDESTRIAN CONNECTIVITY

6.1 BROAD STREET @ SYDNEY STREET

The Study Team completed a review of existing pedestrian infrastructure within the Study Area including the intersection of Broad Street and Sydney Street as well as the section of Sydney Street between Broad Street and the proposed development site. Broad Street currently features an asphalt sidewalk separated by a grass boulevard along the north side of the street. Along the south side of Broad Street an asphalt sidewalk extends east of the intersection and west of the intersection is the Harbour Passage, which was extended along the south side of Broad Street to Sydney Street within the last two years. Despite the convergence of these sidewalks/trails at all four legs of the intersection, there are no marked crosswalks at the Broad Street/Sydney Street intersection.

Per the *TAC Pedestrian Crossing Control Guide, Third Edition*, a signed and marked crosswalk with side-mounted signs is appropriate for Broad Street given that it is projected to have an AADT between 4,500 and 9,000 vehicles per day, a two-lane cross-section, and a speed limit of 50 km/h. Typically, at a two-way stop control intersection only one designated crosswalk would be provided across the free-flow approaches to minimize signage and draw the attention of drivers to only one location where pedestrians are expected to cross. Given the location of the existing sidewalks, it would be most appropriate to install the sidewalk across the west approach on Broad Street.

Traffic volumes on Sydney Street are currently less than 1500 vehicles per day, which is the threshold TAC recommends for a marked crosswalk; however, the proposed development is expected to generate approximately 900 vehicle trips per day, which would increase the AADT past 1500. Therefore, marked crosswalks with side-mounted signs should also be installed at both approaches on Sydney Street.

6.2 SYDNEY STREET SOUTH OF BROAD STREET

Between Broad Street and Broadview Avenue, Sydney Street features an asphalt sidewalk along the west side of the street that is located directly adjacent to the travel lanes. There is no curb and gutter along either side of the street; however, a short section of curb and sidewalk at the southwest corner of the Broad Street/Sydney Street intersection shown below in **Figure 6** suggests that a curb and sidewalk extension is planned down the west side of Sydney Street in the future.

South of Broadview Avenue, the sidewalk extends along the east side of Sydney Street until it reaches the proposed development site, where it ends. The northern section of sidewalk along this stretch is asphalt without curbing, while the southern section is a monolithic concrete sidewalk, as shown in **Figure 7**. Overall, the pedestrian facilities along Sydney Street between Broad Street and the development site feature varying sidewalk types which are not well connected. The infrastructure appears to be the result of a piecemeal approach which likely saw pedestrian infrastructure added in short segments as development progressed along the street.

Figure 6 – Southbound view of Harbour Passage and future sidewalk extension at Broad/Sydney



Figure 7 – Southbound View of Sidewalk on Sydney approaching Development Site



To improve pedestrian connectivity to the development site, upgrades to the pedestrian infrastructure along Sydney Street south of Broad Street should be considered. New concrete sidewalk along the east side of Sydney Street would complement the relatively new concrete sidewalk that exists along the east side of the street immediately north of the development site. Extension of new sidewalk or trail down the west side of the street would also be an option worth considering.

An extension of the Harbour Passage has been planned for several years. The extension down Broad to Sydney Street was considered Phase 1, while Phase 2 is intended to see the Harbour Passage extended down Sydney Street to Tin Can Beach, as shown below in **Figure 8**. This extension would help facilitate an improved pedestrian connection to the development site.

Figure 8 – Harbour Passage Extension



Source: <https://www.cbc.ca/news/canada/new-brunswick/harbour-passage-extension-to-carry-saint-john-walkway-to-crown-street-1.4124415>

7 CONCLUSIONS AND RECOMMENDATIONS

The key findings and recommendations of this Traffic Impact Statement are summarized as follows:

1. The proposed development, located on the south end of Sydney Street, is a 12-storey apartment complex, consisting of 180 dwelling units. The proposed development plan shows 186 parking spaces, including 71 regular and 4 barrier-free surface level parking spaces and 109 regular and 2 barrier-free underground parking spaces. The parking facilities are accessible via two accesses on Sydney Street.
2. It is expected that the proposed development will generate 63 vehicle trips during the AM Peak hour (15 entering/48 exiting) and 70 vehicle trips during the PM Peak hour (43 entering/27 exiting) and a total of 921 trips daily.
3. The LOS results for the 2028 horizon period with the development in place suggest that the trips generated by the proposed development are not expected to significantly impact traffic operations at the intersection of Sydney Street and Broad Street. Vehicles travelling northbound from the development area are expected to experience delays of approximately 15-16 seconds at Broad Street during peak periods.
4. The traffic signal warrant analysis concluded that traffic signals will not be warranted at the intersection of Sydney Street and Broad Street for the 2028 horizon year with the development in place.
5. It was recommended that marked crosswalks equipped with ground-mounted signs be added to the north, west, and south legs of the Sydney Street/Broad Street intersection.
6. It was recommended that pedestrian infrastructure along Sydney Street south of Broad Street be upgraded to improve pedestrian access to the development site. This could include sidewalk reconstruction along the east side of the street or sidewalk/trail upgrades along the west side of the street. The improvements should align with plans to extend the Harbour Passage down Sydney Street to improve access to Tin Can Beach.

Appendix A: Development Site Plans



Not For Construction

ISSUE	DESCRIPTION	DATE
01		

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ANY CHANGES TO THIS DESIGN, PRIOR TO OR DURING CONSTRUCTION, MUST BE APPROVED BY THE ARCHITECT & ARCHITECTURAL DESIGNER.

ALL CONTRACTORS MUST CONFORM TO ALL REGULATIONS, MUNICIPAL AND PROVINCIAL BY-LAWS AND "THE NATIONAL BUILDING CODE OF CANADA".

ALL REQUIRED PERMITS MUST BE OBTAINED PRIOR TO ANY CONSTRUCTION.

Rev.#	Description	Date
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Stamp:

Architectural Consultant:

Architectural Designer:

spitfire
DESIGN CO.
171 Lutz Street, Moncton, NB E1C 5E9
Bus: (506) 855-3777 Cell: (506) 312-2777 eMail: denis@spitfiredesign.ca

Client:

Parsi Development Ltd.

Project:

Sydney St. Multi res.

300 Sydney st., Saint John

Drawing Title:

BASEMENT

Date: Feb 1 2020

Checked by: JP

Drawn by: EM Revision: -

Scale: AS NOTED

Sheet: ACO.2 Flight no: 4334

SUITE BREAKDOWN PER FLOOR					
Name	Comments	Area	Level	Count	
2F:					
SUITE A	3 BED, 2 BATH, DEN	1440 SF	2F	1	
SUITE B	3 BED, 2 BATH, DEN	1550 SF	2F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	2F	4	
SUITE D	1 BED, 1 BATH	850 SF	2F	5	
SUITE E	1 BED, 1 BATH	940 SF	2F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	2F	1	
SUITE G	3 BED, 2 BATH, DEN	1400 SF	2F	2	
SUITE I	1 BED, 1 BATH, DEN	850 SF	2F	1	
SUITE J	1 BED, 1 BATH	820 SF	2F	1	
SUITE K	1 BED, 1 BATH	790 SF	2F	1	
SUITE L	3 BED, 2 BATH	1410 SF	2F	1	
SUITE M	2 BED, 2 BATH, DEN	1290 SF	2F	1	

3F:					
SUITE A	3 BED, 2 BATH, DEN	1470 SF	3F	1	
SUITE B	3 BED, 2 BATH, DEN	1550 SF	3F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	3F	4	
SUITE D	1 BED, 1 BATH	850 SF	3F	5	
SUITE E	1 BED, 1 BATH	940 SF	3F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	3F	1	
SUITE G	3 BED, 2 BATH, DEN	1400 SF	3F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	3F	1	
SUITE J	1 BED, 1 BATH	820 SF	3F	1	
SUITE K	1 BED, 1 BATH	800 SF	3F	1	
SUITE L	3 BED, 2 BATH	1420 SF	3F	1	
SUITE M	2 BED, 2 BATH, DEN	1320 SF	3F	1	

4F:					
SUITE A	3 BED, 2 BATH, DEN	1470 SF	4F	1	
SUITE B	3 BED, 2 BATH, DEN	1550 SF	4F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	4F	4	
SUITE D	1 BED, 1 BATH	850 SF	4F	5	
SUITE E	1 BED, 1 BATH	960 SF	4F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	4F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	4F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	4F	1	
SUITE J	1 BED, 1 BATH	840 SF	4F	1	
SUITE K	1 BED, 1 BATH	800 SF	4F	1	
SUITE L	3 BED, 2 BATH	1420 SF	4F	1	
SUITE M	2 BED, 2 BATH, DEN	1320 SF	4F	1	

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SUITE B	3 BED, 2 BATH, DEN	1550 SF	5F	1	
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SUITE F	1 BED, 1 BATH, DEN	1260 SF	5F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	5F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	5F	1	
SUITE J	1 BED, 1 BATH	840 SF	5F	1	
SUITE K	1 BED, 1 BATH	800 SF	5F	1	
SUITE L	3 BED, 2 BATH	1420 SF	5F	1	
SUITE M	2 BED, 2 BATH, DEN	1320 SF	5F	1	

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SUITE B	3 BED, 2 BATH, DEN	1550 SF	6F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	6F	4	
SUITE D	1 BED, 1 BATH	850 SF	6F	5	
SUITE E	1 BED, 1 BATH	960 SF	6F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	6F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	6F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	6F	1	
SUITE J	1 BED, 1 BATH	840 SF	6F	1	
SUITE K	1 BED, 1 BATH	800 SF	6F	1	
SUITE L	3 BED, 2 BATH	1420 SF	6F	1	
SUITE M	2 BED, 2 BATH, DEN	1290 SF	6F	1	

Grand total: 180

SUITE BREAKDOWN PER FLOOR					
Name	Comments	Area	Level	Count	
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SUITE A	3 BED, 2 BATH, DEN	1440 SF	7F	1	
SUITE B	3 BED, 2 BATH, DEN	1550 SF	7F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	7F	4	
SUITE D	1 BED, 1 BATH	850 SF	7F	5	
SUITE E	1 BED, 1 BATH	960 SF	7F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	7F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	7F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	7F	1	
SUITE J	1 BED, 1 BATH	840 SF	7F	1	
SUITE K	1 BED, 1 BATH	800 SF	7F	1	
SUITE L	3 BED, 2 BATH	1420 SF	7F	1	
SUITE M	2 BED, 2 BATH, DEN	1290 SF	7F	1	

8F:					
SUITE A	3 BED, 2 BATH, DEN	1440 SF	8F	1	
SUITE B	3 BED, 2 BATH, DEN	1550 SF	8F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	8F	4	
SUITE D	1 BED, 1 BATH	850 SF	8F	5	
SUITE E	1 BED, 1 BATH	960 SF	8F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	8F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	8F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	8F	1	
SUITE J	1 BED, 1 BATH	840 SF	8F	1	
SUITE K	1 BED, 1 BATH	800 SF	8F	1	
SUITE L	3 BED, 2 BATH	1420 SF	8F	1	
SUITE M	2 BED, 2 BATH, DEN	1290 SF	8F	1	

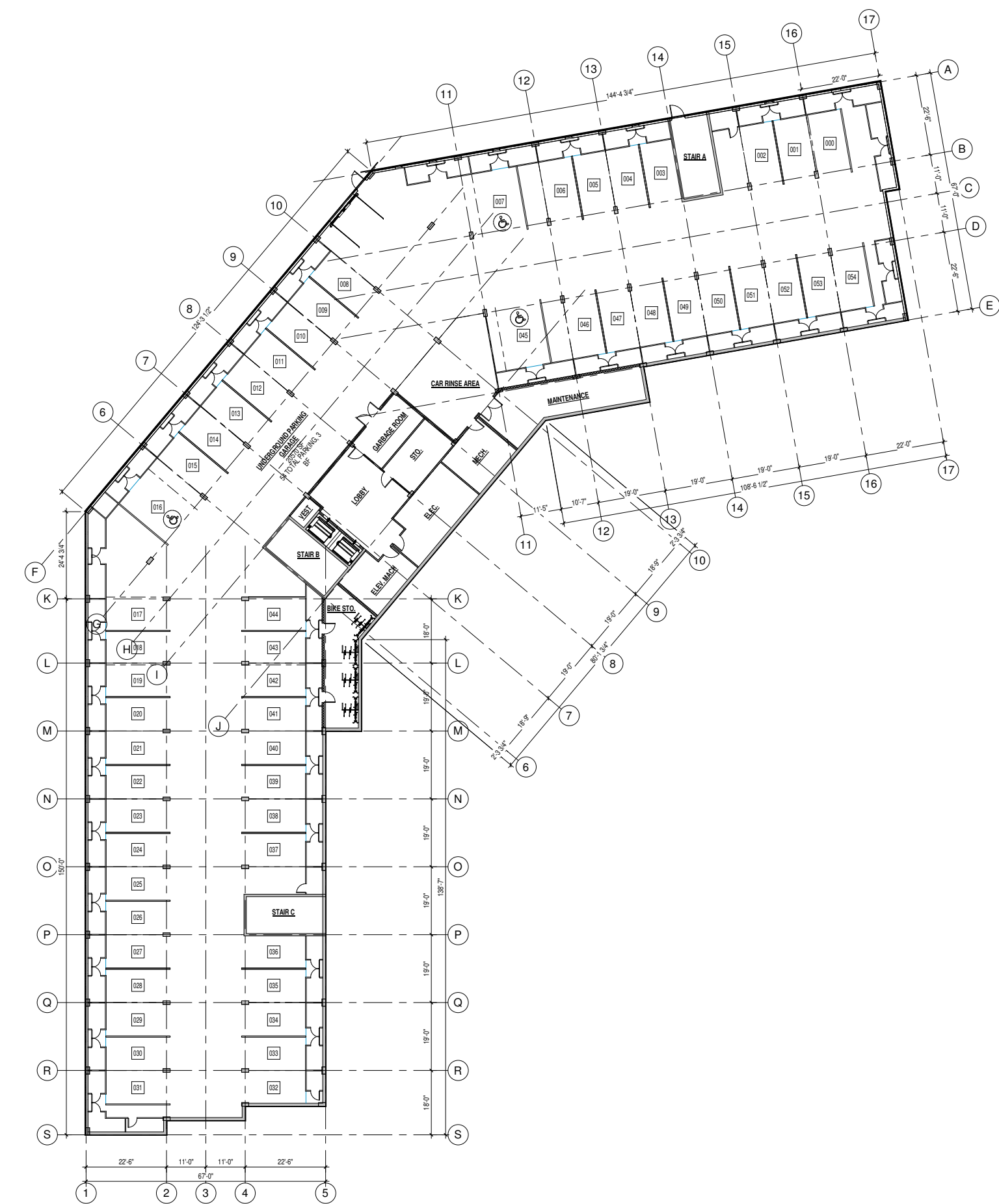
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SUITE D	1 BED, 1 BATH	850 SF	9F	5	
SUITE E	1 BED, 1 BATH	960 SF	9F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	9F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	9F	2	
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SUITE J	1 BED, 1 BATH	840 SF	9F	1	
SUITE K	1 BED, 1 BATH	800 SF	9F	1	
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SUITE M	2 BED, 2 BATH, DEN	1290 SF	9F	1	

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SUITE B	3 BED, 2 BATH, DEN	1550 SF	10F	1	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	10F	4	
SUITE D	1 BED, 1 BATH	850 SF	10F	5	
SUITE E	1 BED, 1 BATH	960 SF	10F	1	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	10F	1	
SUITE G	3 BED, 2 BATH, DEN	1420 SF	10F	2	
SUITE I	1 BED, 1 BATH, DEN	870 SF	10F	1	
SUITE J	1 BED, 1 BATH	840 SF	10F	1	
SUITE K	1 BED, 1 BATH	800 SF	10F	1	
SUITE L	3 BED, 2 BATH	1420 SF	10F	1	
SUITE M	2 BED, 2 BATH, DEN	1290 SF	10F	1	

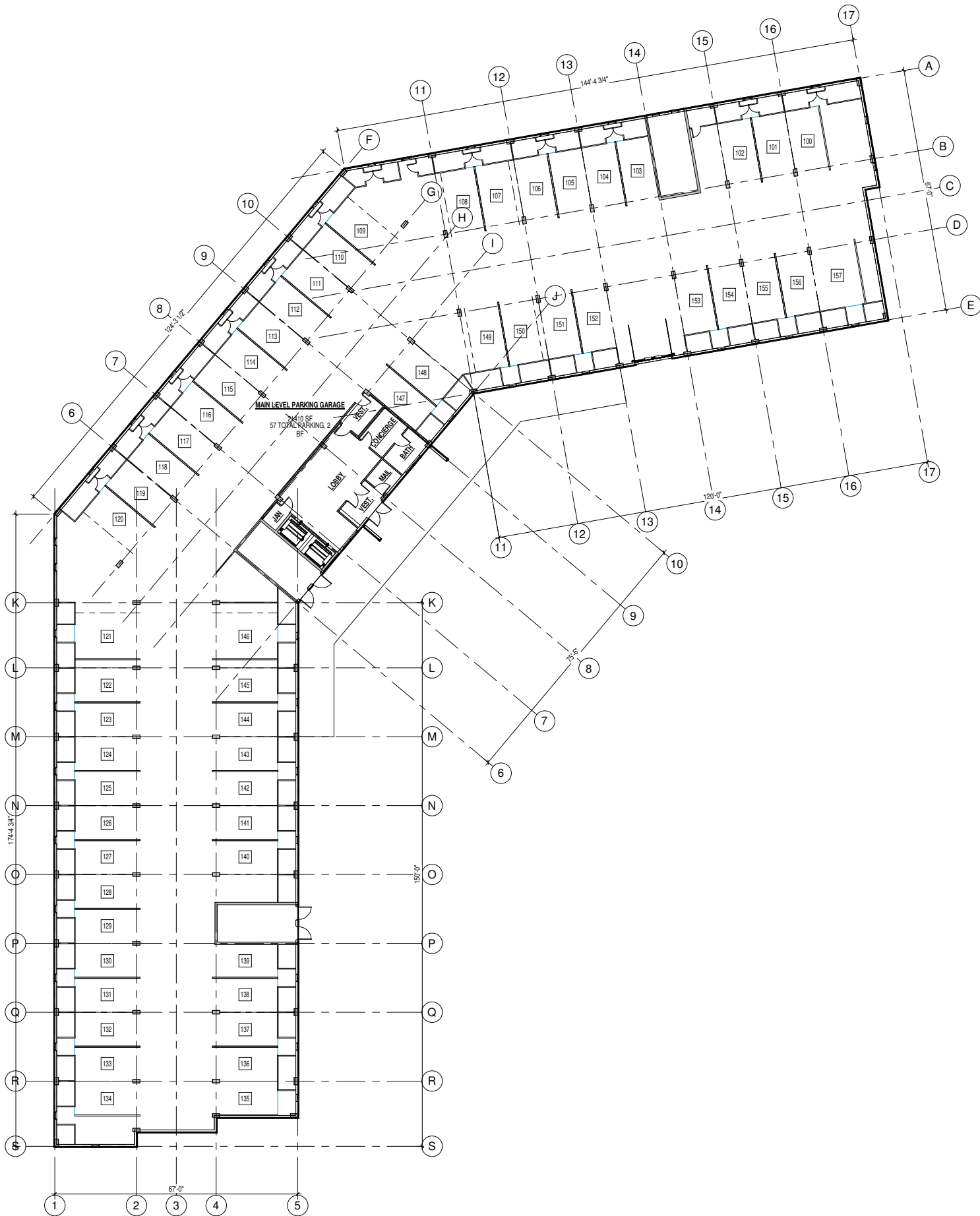
Grand total: 180

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SUITE F	1 BED, 1 BATH, DEN	1260 SF	9
SUITE G	3 BED, 2 BATH, DEN	1400 SF	18
SUITE I	1 BED, 1 BATH, DEN	850 SF	9
SUITE J	1 BED, 1 BATH	820 SF	9
SUITE K	1 BED, 1 BATH	790 SF	9
SUITE L	3 BED, 2 BATH	1410 SF	9
SUITE M	2 BED, 2 BATH, DEN	1290 SF	9

TOTAL BEDROOMS: 315



UNDERGROUND PARKING
1/16" = 1'-0"



MAIN FLOOR
1/16" = 1'-0"

SUITE BREAKDOWN PER FLOOR				
Name	Comments	Area	Level	Count

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SUITE B	3 BED, 2 BATH, DEN	1550 SF	2F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	2F	4
SUITE D	1 BED, 1 BATH	850 SF	2F	5
SUITE E	1 BED, 1 BATH	940 SF	2F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	2F	1
SUITE G	3 BED, 2 BATH, DEN	1400 SF	2F	2
SUITE I	1 BED, 1 BATH, DEN	850 SF	2F	1
SUITE J	1 BED, 1 BATH	820 SF	2F	1
SUITE K	1 BED, 1 BATH	790 SF	2F	1
SUITE L	3 BED, 2 BATH	1410 SF	2F	1
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3F:				
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SUITE C	2 BED, 2 BATH, DEN	1120 SF	3F	4
SUITE D	1 BED, 1 BATH	850 SF	3F	5
SUITE E	1 BED, 1 BATH	940 SF	3F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	3F	1
SUITE G	3 BED, 2 BATH, DEN	1400 SF	3F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	3F	1
SUITE J	1 BED, 1 BATH	820 SF	3F	1
SUITE K	1 BED, 1 BATH	800 SF	3F	1
SUITE L	3 BED, 2 BATH	1420 SF	3F	1
SUITE M	2 BED, 2 BATH, DEN	1320 SF	3F	1
3F: 20				

4F:				
SUITE A	3 BED, 2 BATH, DEN	1470 SF	4F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	4F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	4F	4
SUITE D	1 BED, 1 BATH	850 SF	4F	5
SUITE E	1 BED, 1 BATH	960 SF	4F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	4F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	4F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	4F	1
SUITE J	1 BED, 1 BATH	840 SF	4F	1
SUITE K	1 BED, 1 BATH	800 SF	4F	1
SUITE L	3 BED, 2 BATH	1420 SF	4F	1
SUITE M	2 BED, 2 BATH, DEN	1320 SF	4F	1
4F: 20				

5F:				
SUITE A	3 BED, 2 BATH, DEN	1470 SF	5F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	5F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	5F	4
SUITE D	1 BED, 1 BATH	850 SF	5F	5
SUITE E	1 BED, 1 BATH	960 SF	5F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	5F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	5F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	5F	1
SUITE J	1 BED, 1 BATH	840 SF	5F	1
SUITE K	1 BED, 1 BATH	800 SF	5F	1
SUITE L	3 BED, 2 BATH	1420 SF	5F	1
SUITE M	2 BED, 2 BATH, DEN	1320 SF	5F	1
5F: 20				

6F:				
SUITE A	3 BED, 2 BATH, DEN	1440 SF	6F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	6F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	6F	4
SUITE D	1 BED, 1 BATH	850 SF	6F	5
SUITE E	1 BED, 1 BATH	960 SF	6F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	6F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	6F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	6F	1
SUITE J	1 BED, 1 BATH	840 SF	6F	1
SUITE K	1 BED, 1 BATH	800 SF	6F	1
SUITE L	3 BED, 2 BATH	1420 SF	6F	1
SUITE M	2 BED, 2 BATH, DEN	1290 SF	6F	1
6F: 20				

SUITE BREAKDOWN PER FLOOR				
Name	Comments	Area	Level	Count

7F:				
SUITE A	3 BED, 2 BATH, DEN	1440 SF	7F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	7F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	7F	4
SUITE D	1 BED, 1 BATH	850 SF	7F	5
SUITE E	1 BED, 1 BATH	960 SF	7F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	7F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	7F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	7F	1
SUITE J	1 BED, 1 BATH	840 SF	7F	1
SUITE K	1 BED, 1 BATH	800 SF	7F	1
SUITE L	3 BED, 2 BATH	1420 SF	7F	1
SUITE M	2 BED, 2 BATH, DEN	1290 SF	7F	1
7F: 20				

8F:				
SUITE A	3 BED, 2 BATH, DEN	1440 SF	8F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	8F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	8F	4
SUITE D	1 BED, 1 BATH	850 SF	8F	5
SUITE E	1 BED, 1 BATH	960 SF	8F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	8F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	8F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	8F	1
SUITE J	1 BED, 1 BATH	840 SF	8F	1
SUITE K	1 BED, 1 BATH	800 SF	8F	1
SUITE L	3 BED, 2 BATH	1420 SF	8F	1
SUITE M	2 BED, 2 BATH, DEN	1290 SF	8F	1
8F: 20				

9F:				
SUITE A	3 BED, 2 BATH, DEN	1440 SF	9F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	9F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	9F	4
SUITE D	1 BED, 1 BATH	850 SF	9F	5
SUITE E	1 BED, 1 BATH	960 SF	9F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	9F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	9F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	9F	1
SUITE J	1 BED, 1 BATH	840 SF	9F	1
SUITE K	1 BED, 1 BATH	800 SF	9F	1
SUITE L	3 BED, 2 BATH	1420 SF	9F	1
SUITE M	2 BED, 2 BATH, DEN	1290 SF	9F	1
9F: 20				

10F:				
SUITE A	3 BED, 2 BATH, DEN	1440 SF	10F	1
SUITE B	3 BED, 2 BATH, DEN	1550 SF	10F	1
SUITE C	2 BED, 2 BATH, DEN	1120 SF	10F	4
SUITE D	1 BED, 1 BATH	850 SF	10F	5
SUITE E	1 BED, 1 BATH	960 SF	10F	1
SUITE F	1 BED, 1 BATH, DEN	1260 SF	10F	1
SUITE G	3 BED, 2 BATH, DEN	1420 SF	10F	2
SUITE I	1 BED, 1 BATH, DEN	870 SF	10F	1
SUITE J	1 BED, 1 BATH	840 SF	10F	1
SUITE K	1 BED, 1 BATH	800 SF	10F	1
SUITE L	3 BED, 2 BATH	1420 SF	10F	1
SUITE M	2 BED, 2 BATH, DEN	1290 SF	10F	1
10F: 20				

Grand total: 180				
SUITE BREAKDOWN				
Name	Comments	Area	Count	
SUITE A	3 BED, 2 BATH, DEN	1440 SF	9	
SUITE B	3 BED, 2 BATH, DEN	1550 SF	9	
SUITE C	2 BED, 2 BATH, DEN	1120 SF	36	
SUITE D	1 BED, 1 BATH	850 SF	45	
SUITE E	1 BED, 1 BATH	940 SF	9	
SUITE F	1 BED, 1 BATH, DEN	1260 SF	9	
SUITE G	3 BED, 2 BATH, DEN	1400 SF	18	
SUITE I	1 BED, 1 BATH, DEN	850 SF	9	
SUITE J	1 BED, 1 BATH	820 SF	9	
SUITE K	1 BED, 1 BATH	790 SF	9	
SUITE L	3 BED, 2 BATH	1410 SF	9	
SUITE M	2 BED, 2 BATH, DEN	1290 SF	9	
Grand total: 180				



"Not For Construction"

ISSUE	DESCRIPTION	DATE
01		

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Rev.#	Description	Date
-------	-------------	------

Stamp:	

Architectural Consultant:	

Architectural Designer:	

Client:	Parsi Development Ltd.
---------	-------------------------------

Project:	Sydney St. Multi res.
	300 Sydney st., Saint John

Drawing Title:	MAIN FLOOR
----------------	------------

Date:	Feb 1 2020
Checked by:	JP
Drawn by:	EM
Scale:	AS NOTED
Sheet:	AC0.3
Flight no:	4334



Sheet:	ACO.4	Flight no:	4334
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Grand total: 180

LEVEL 2 TO LEVEL 5
1/16" = 1'-0"



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Architectural Consultant:

Architectural Designer:



Parsi Development Ltd.

Sydney St. Multi res.

300 Sydney st., Saint John

Drawing Title:

LEVEL 11

Date:

Feb 1 2020

Checked by:

JP

Drawn by:

EM

Scale:

AS NOTED

Sheet:

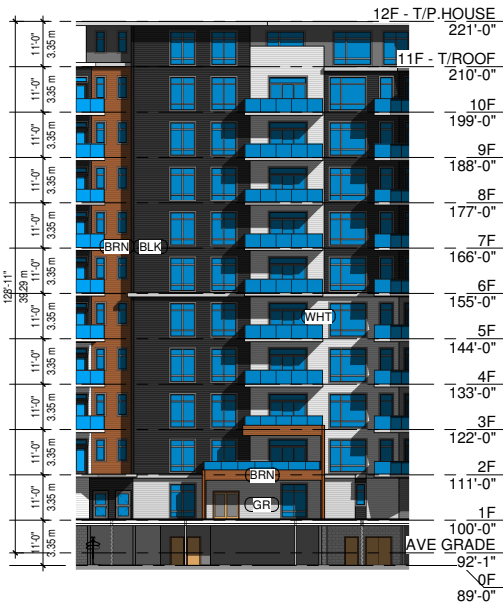
ACO.5

Flight no: 4334


$$\overline{1/16'' = 1'-0''}$$



LEFT WING - INSIDE
3/64" = 1'-0"



INSIDE CORNER CORE
3/64" = 1'-0"



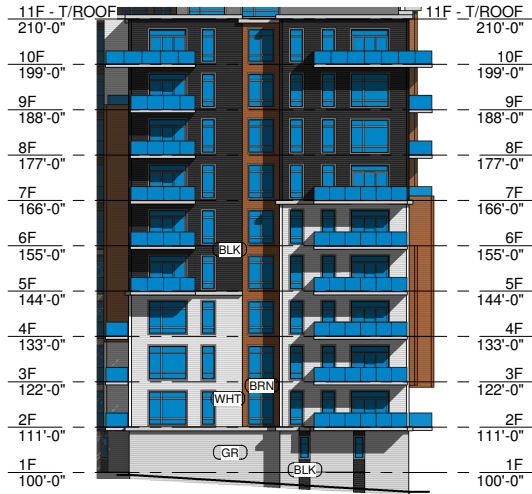
RIGHT WING - INSIDE
3/64" = 1'-0"



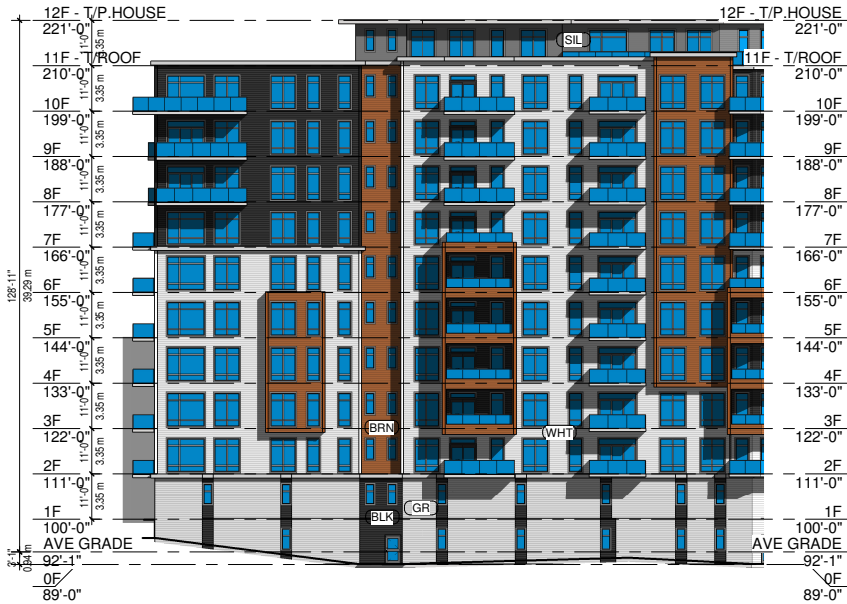
LEFT WING - END
3/64" = 1'-0"

MATERIALS:
EXACT MATERIAL COLORS SPECIFIED BY OWNER
BLK- BLACK CLADDING, NON-COMBUSTIBLE
WHT- WHITE CLADDING, NON-COMBUSTIBLE
GR- GRAY CLADDING, NON-COMBUSTIBLE
BRN- BROWN CLADDING, NON-COMBUSTIBLE
SIL- SILVER CLADDING- NON-COMBUSTIBLE

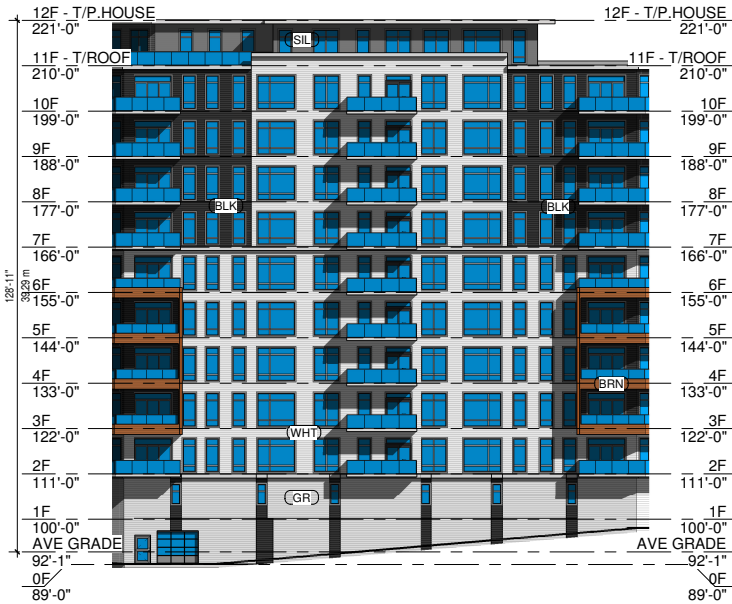
MATERIAL LEGEND
1/4" = 1'-0"



RIGHT WING END
3/64" = 1'-0"



RIGHT WING - OUTSIDE
3/64" = 1'-0"



OUTSIDE CORNER
3/64" = 1'-0"



LEFT WING - OUTSIDE
3/64" = 1'-0"



"Not For Construction"

ISSUE	DESCRIPTION	DATE
01		

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ALL REQUIRED PERMITS MUST BE OBTAINED PRIOR TO ANY CONSTRUCTION.

Rev.#	Description	Date

Stamp:

Architectural Consultant:

Architectural Designer:

*** spitfire ***
DESIGN CO.
171 Lutz Street, Moncton, NB E1C 5E9
Bus: (506) 855-3777 Cell: (506) 312-2777 eMail: denis@spitfiredesign.ca

Client: **Parsi Development Ltd.**

Project: **Sydney St. Multi res.**
300 Sydney st., Saint John

Drawing Title: **ELEVATIONS**

Date: Feb 1 2020

Checked by: JP

Drawn by: EM Revision: -

Scale: AS NOTED

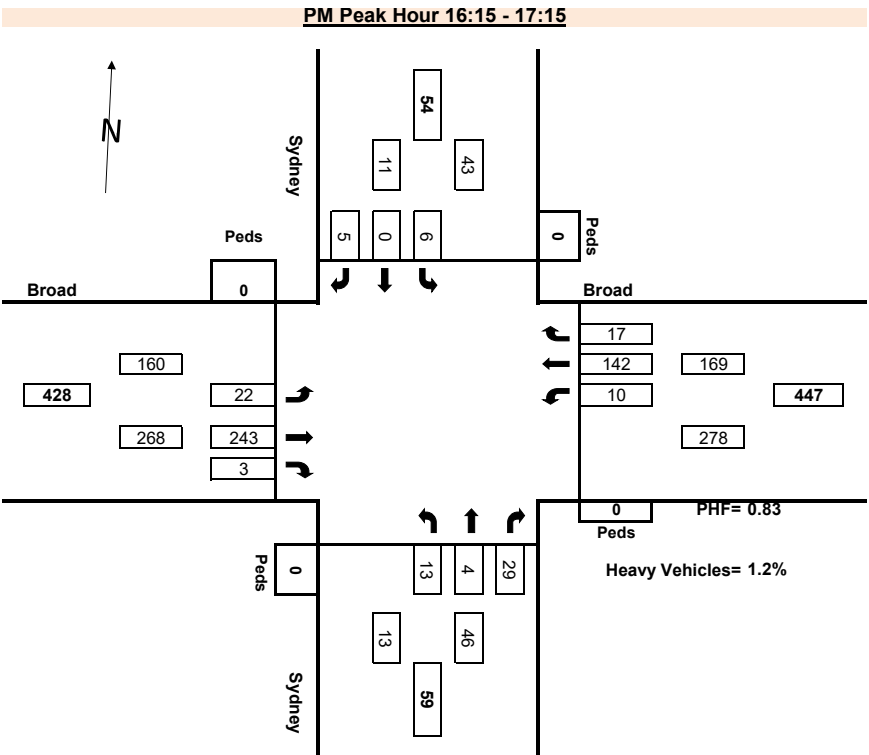
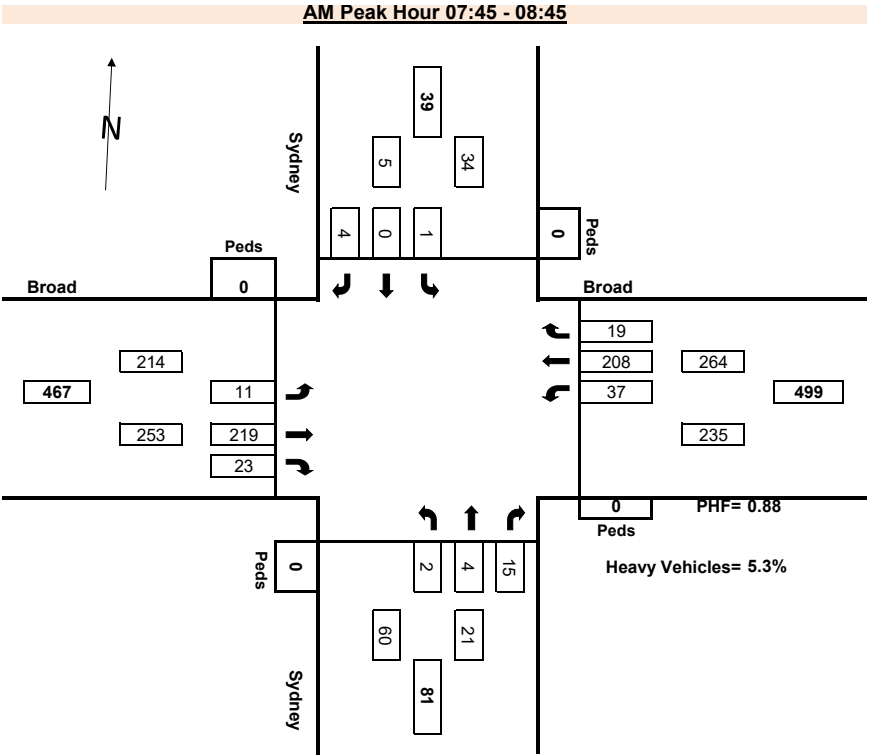
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Appendix B: Traffic Count Data

Traffic Count Summary

AM and PM Peak Hours

Sydney @ Broad



















Appendix C: Level of Service Reports

Sydney St TIS
3: Sydney & Broad

2028 AM Peak with Development

















02-17-2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	15	307	39	59	291	27	24	8	45	1	0	5
Future Volume (Veh/h)	15	307	39	59	291	27	24	8	45	1	0	5
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Hourly flow rate (vph)	17	349	44	67	331	31	27	9	51	1	0	6
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type	None			None								
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	362			393			892	901	371	941	908	346
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	362			393			892	901	371	941	908	346
tC, single (s)	4.2			4.1			7.1	6.5	6.3	7.1	6.5	6.5
tC, 2 stage (s)												
tF (s)	2.3			2.2			3.5	4.0	3.4	3.5	4.0	3.5
p0 queue free %	99			94			89	97	92	100	100	99
cM capacity (veh/h)	1159			1160			248	260	664	209	258	647
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	410	429	87	7								
Volume Left	17	67	27	1								
Volume Right	44	31	51	6								
cSH	1159	1160	395	498								
Volume to Capacity	0.01	0.06	0.22	0.01								
Queue Length 95th (m)	0.4	1.5	6.6	0.3								
Control Delay (s)	0.5	1.8	16.7	12.3								
Lane LOS	A	A	C	B								
Approach Delay (s)	0.5	1.8	16.7	12.3								
Approach LOS				C	B							
Intersection Summary												
Average Delay				2.7								
Intersection Capacity Utilization				56.0%	ICU Level of Service	B						
Analysis Period (min)				15								

Sydney St TIS
3: Sydney & Broad

2028 PM Peak with Developmen

02-17-2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	29	317	30	29	185	22	26	7	53	7	1	6
Future Volume (Veh/h)	29	317	30	29	185	22	26	7	53	7	1	6
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83
Hourly flow rate (vph)	35	382	36	35	223	27	31	8	64	8	1	7
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type	None			None								
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	250			418			784	790	400	844	794	236
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	250			418			784	790	400	844	794	236
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	97			97			90	97	90	97	100	99
cM capacity (veh/h)	1298			1152			296	306	654	241	305	807
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	453	285	103	16								
Volume Left	35	35	31	8								
Volume Right	36	27	64	7								
cSH	1298	1152	451	355								
Volume to Capacity	0.03	0.03	0.23	0.05								
Queue Length 95th (m)	0.7	0.8	7.0	1.1								
Control Delay (s)	0.9	1.3	15.3	15.6								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.9	1.3	15.3	15.6								
Approach LOS			C	C								
Intersection Summary												
Average Delay			3.0									
Intersection Capacity Utilization			36.2%		ICU Level of Service				A			
Analysis Period (min)			15									

Appendix D: Signal Warrant Worksheet

City of Saint John - Traffic Signal & Pedestrian Signal Head Warrant Analysis

Main Street (name)	Broad Street	Direction (EW or NS) EW NS Comments: 2028 Volumes with Development Traffic (and COVID Adjustments)	Road Authority:	City of Saint John
Side Street (name)	Sydney Street		City:	Saint John
Quadrant / Int #			Analysis Date:	2021 Feb 12, Fri
or Warrant Calculation Results, please hit 'Page Down'	CHECK SHEET		Count Date:	20201-02-09
			Date Entry Format:	(yyyy-mm-dd)

Lane Configuration			Excl LT	Th & LT	Through	Th+RT+LT	Th & RT	Excl RT	RT	Channelization (y/n)	UpStream Signal (m)	# of Thru Lanes	LT Phase Type	RTOR Allowed (y/n)	Actuated Thru Phase
	Broad Street	WB				1					1,000	1	perm	y	y
	Broad Street	EB				1					1,000	1	perm	y	y
	Sydney Street	NB				1					1,000	1	perm	y	y
	Sydney Street	SB				1					1,000	1	perm	y	y

Saturation Flow Rates (if not default) (vphpl)		Default Saturation Flow Rates (vphpl)
Left Turn		1,650
Through		1,800
Right Turn		1,500

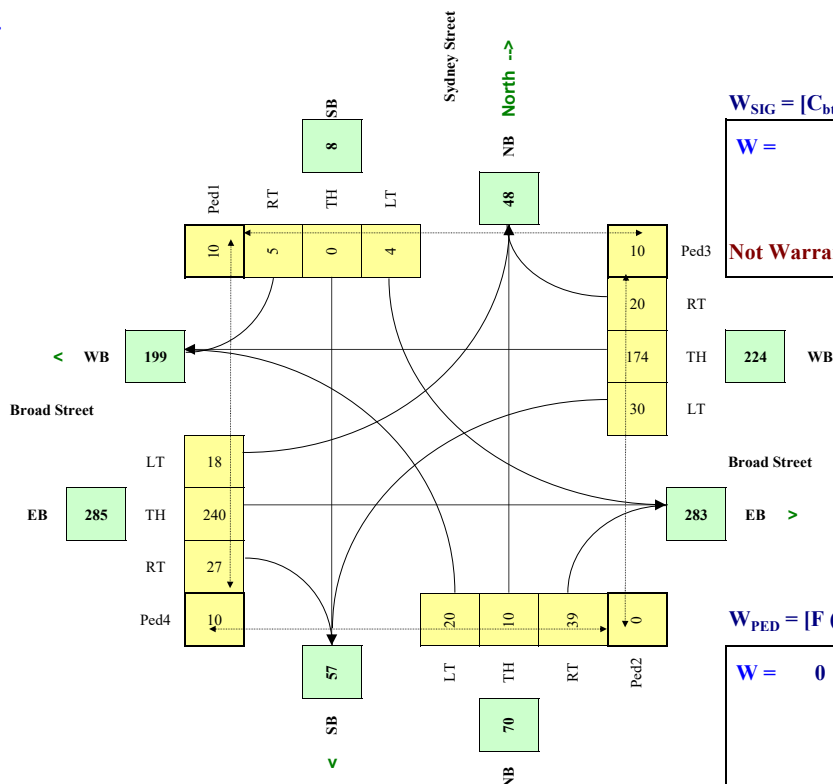
Are the Sydney Street NB right turns significantly impeded by through movements?	(y/n)	n
Are the Sydney Street SB right turns significantly impeded by through movements?	(y/n)	n
Are the Broad Street WB right turns significantly impeded by through movements?	(y/n)	n
Are the Broad Street EB right turns significantly impeded by through movements?	(y/n)	n

Demographics		
Elem. School/Mobility Challenged	(y/n)	n
Senior's Complex	(y/n)	n
Pathway to School	(y/n)	n
Metro Area Population	(#)	70,785
Central Business District	(y/n)	v

Other input		Speed (Km/h)	Truck %	Bus Rt (y/n)	Median (m)
Broad Street	EW	50	4.0%	y	0.0
Sydney Street	NS	50	3.0%	n	0.0

Sydney Street	NS	30	100%	11	0.0														
Set Peak Hours															Ped1	Ped2	Ped3	Ped4	
Traffic Input	NB			SB			WB			EB			NS	NS	EW	EW			
	LT	Th	RT	LT	Th	RT	LT	Th	RT	LT	Th	RT	W Side	E Side	N Side	S Side			
	13	5	16	1	0	3	14	113	19	5	164	14	10		10	10			
	22	8	42	0	0	5	62	302	17	18	291	40	10		10	10			
	23	11	47	5	0	5	24	151	14	18	225	21	10		10	10			
	24	15	36	5	0	2	30	151	12	22	223	29	10		10	10			
	15	9	44	7	0	7	26	174	22	29	322	31	10		10	10			
	25	14	50	5	0	5	23	152	35	17	213	27	10		10	10			
	Total (6-hour peak)	122	62	235	23	0	27	179	1,043	119	109	1,438	162	60	0	60	60		
Average (6-hour peak)	20	10	39	4	0	5	30	174	20	18	240	27	10	0	10	10			
Actual Pedestrian Crossing Distance (m)																			

Average 6-hour Peak Turning Movements



$$W_{SIG} = [C_{bt}(X_{v-v}) / K_1 + (F(X_{v-p}) L) / K_2] \times C_i$$

W =	24	19	5
		<i>Veh</i>	<i>Ped</i>

Not Warranted - Vs<75

RESET SHEET

$$\mathbf{W}_{\text{PED}} = [\mathbf{F} ((\mathbf{X}_{\text{ped}_m})\mathbf{d}_m/\mathbf{K}_2) + (\mathbf{X}_{\text{ped}_s})\mathbf{d}_s/\mathbf{K}_3]$$

$$\mathbf{W} = \mathbf{0}$$

Not Warranted - Ped Vol<25(avg)